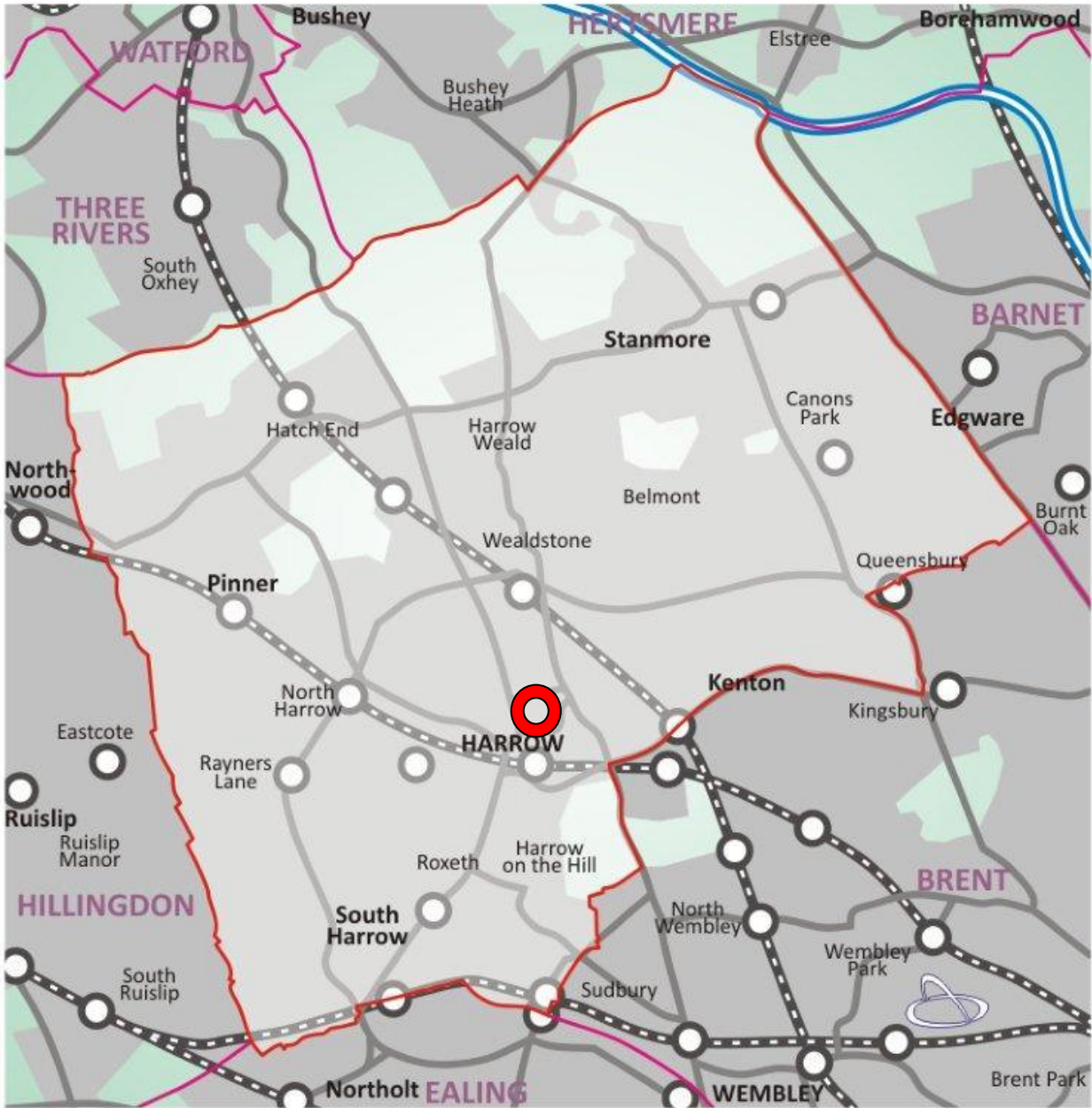
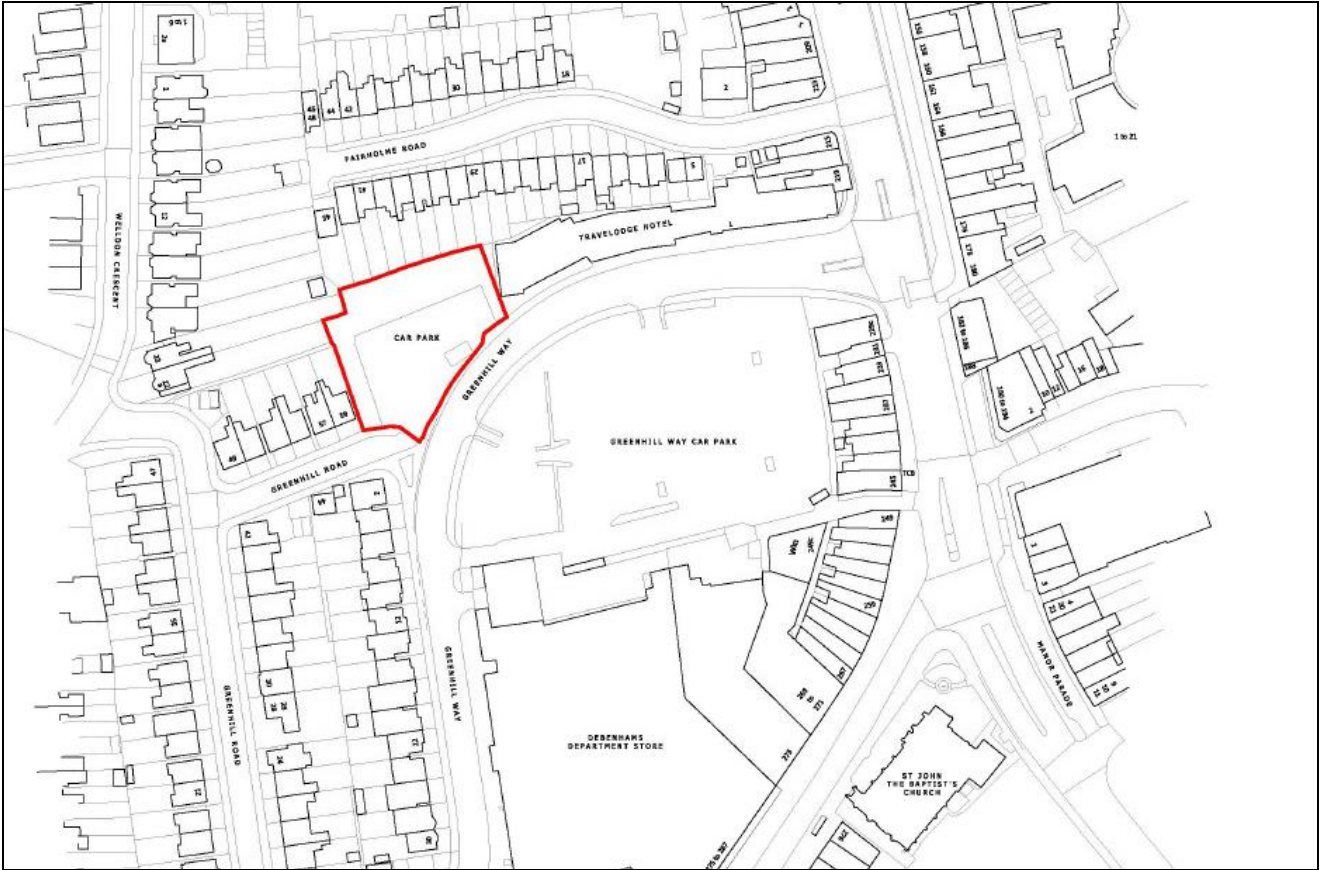


 = application site



North Side Car Park, Greenhill Way, HA1 1LE	P/1257/20
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NORTH SIDE CAR PARK, GREENHILL WAY



LONDON BOROUGH OF HARROW

PLANNING COMMITTEE

14th October 2020

APPLICATION NUMBER: P/1257/20
VALIDATE DATE: 6th APRIL 2020
LOCATION: NORTH SIDE CAR PARK, GREENHILL WAY
WARD: GREENHILL
POSTCODE: HA5 1LE
APPLICANT: MS EVA SISKINOVA
AGENT: DANDI LIVING LTD
CASE OFFICER: SELINA HOTWANI
EXPIRY DATE: 4th SEPTEMBER 2020
(Extended Expiry: 15th DECEMBER 2020)

PROPOSAL

Redevelopment to create a part 3, part 4 and part 5 storey building to provide 89 co-living units with communal facilities; landscaping; parking; refuse and cycle storage

The Planning Committee is asked to:

RECOMMENDATION

- 1) Agree the reasons for approval as set out in this report, and
- 2) Grant planning permission subject to authority being delegated to the Interim Chief Planning Officer in consultation with the Director of Legal and Governance Services for the completion of the Section 106 legal agreement and other enabling legislation and issue of the planning permission and subject to minor amendments to the conditions (set out in Appendix 1 of this report) or the legal agreement. The Section 106 Agreement Heads of Terms would cover the following matters:
 1. **Tenure and Management**
 - Tenure and Management Strategy.
 2. **Affordable Housing Contribution and Viability Review**
 - Early Stage Review.
 - Commuted Sum - £393,647.
 - To pay the affordable housing contribution prior to the commencement of development.
 3. **Carbon Offset**
 - Requiring payment of the carbon offset contribution (£65,672) prior to commencement of development.
 - Post-construction assessment to determine if the proposed carbon reductions have been achieved and whether an additional carbon offset

contribution (residential element) is required to offset any shortfall, calculated at £1,800 per tonne of carbon.

4. Energy Centre

- Provision of a safeguarding route to connect to any future wider District Heat Network.

5. Highways and Parking

Travel Plan Monitoring

- Baseline Survey to be completed within 6 months of agreement (exact options to be secured by agreement) and surveys to be conducted in year 1, 3 and 5 of the development, following the baseline survey.
- Submission of financial costs associated with the Travel Plan are to be provided by the developer.
- Submission of an updated Travel Plan based on the framework travel plan prior to occupation and to cover an initial monitoring period of 5 years.
- Travel Plan Monitoring fees/bond to be secured with the latter only to be used in the event of the development not meeting targets in year 5.
- Appointment of a travel plan co-ordinator.

Highways Agreement

- Section 278 Agreement - for removal of existing footway / kerb outside the Greenhill Road access and resurfacing as carriageway.
- Construction of new kerb and footway to adjoin the existing.
- Improvements to vehicle and pedestrian access of Greenhill Road.

6. Employment and Training - Construction Employment

- The developer to submit to the Council for approval, prior to commencement of the development, a Training and Recruitment Plan/ Local Labour Employment Strategy. The developer to implement the agreed Plan. The training and Employment plan will include:
 - a) employment initiatives opportunities relating to the construction of the Development and details of sector delivery;
 - b) the provision of appropriate training with the objectives of ensuring effective transition into work and sustainable job outcomes;
 - c) the timings and arrangements for implementation of such initiatives and
 - d) suitable mechanisms for the monitoring of the effectiveness of such initiatives
- The developer to use all reasonable endeavours to secure the use of local suppliers and apprentices during the construction of the development.
- A financial contribution towards the management and delivery of the construction training programme based on the construction value of the development. This is calculated using the formula: £2,500 per £1,000,000 build cost.

7. Legal Costs and Monitoring Fee

- Legal fees: A financial contribution (to be agreed) to be paid by the developer to the Council to reimburse the Council's legal costs associated with the preparation of the planning obligation.
- Planning Administration Fee: Payment of £1580 administration fee for the monitoring and compliance of the legal agreement

RECOMMENDATION B

That if the Section 106 Agreement is not completed by **15 December 2020** or as such extended period as may be agreed by the Interim Chief Planning Officer, then it is recommended to delegate the decision to REFUSE planning permission to the Interim Chief Planning Officer on the grounds that:

The proposed development, in the absence of a Legal Agreement to provide appropriate improvements, benefits and monitoring that directly relate to the development, would fail to adequately mitigate the impact of the development on the wider area and provide for necessary social, environmental and physical infrastructural improvements arising directly from the development, contrary to the National Planning Policy Framework (2019), policies 3.11, 3.13, 5.2, 6.3, 7.4, 7.5 and 7.6 of The London Plan (2016), Core Strategy (2012) policy CS1, policies AAP3, AAP13 and AAP19 of the Harrow and Wealdstone Area Action Plan(2013) and policies DM1, DM2 DM42, DM43 and DM50 of the Harrow Development Management Polices Local Plan and the Supplementary Planning Document: Planning Obligations (2013).

REASON FOR THE RECOMMENDATIONS

The proposed development would deliver additional housing of a satisfactory layout and design to ensure that the future occupiers would benefit from a high standard of living accommodation. The redevelopment of the site would enhance the urban environment in terms of material presence, attractive streetscape, and accessible routes in and out of the site and would make a positive contribution to the local area, in terms of quality and character. The massing and scale proposed would appropriately relate to the wider area and would permit full optimisation of this previously developed land to bring forward much needed housing which would positively add to the Council's housing delivery targets. Accordingly, the development would accord with development plan policies and is recommended for approval.

INFORMATION

This application is reported to Planning Committee as it would result in a development of more than 400 sqm of non-residential floorspace. The application is therefore referred to the Planning Committee as it does not fall within any of the provisions set out at paragraphs 1(a) – 1(h) of the Scheme of Delegation dated 12th December 2018.

Statutory Return Type:	All other Largescale Major Development
Council Interest:	None
Net Floorspace:	3,147 sqm
GLA Community Infrastructure Levy (CIL) Contribution	£188,820
Local CIL requirement (provisional):	£173,085

HUMAN RIGHTS ACT

The provisions of the Human Rights Act 1998 have been taken into account in the processing of the application and the preparation of this report.

EQUALITIES

In determining this planning application the Council has regard to its equalities obligations including its obligations under section 149 of the Equality Act 2010.

For the purposes of this application there are no adverse equalities issues.

S17 Crime & Disorder Act

Policies 7.3.B and 7.13.B of The London Plan and Policy DM1 of the Development Management Policies Local Plan require all new developments to have regard to safety and the measures to reduce crime in the design of development proposal. It is considered that the development does not adversely affect crime risk. However, a condition has been recommended for evidence of certification of Secure by Design Accreditation for the development to be submitted to and approved in writing by the Local Planning Authority before any part of the development is occupied or used.

1.0 SITE DESCRIPTION

- 1.1 The application site comprises an existing vacant site although it was formerly used as a pay and display car park accommodating 42 parking spaces and a car wash (sui generis) off Greenhill Way. The site is irregular in shape and comprises approximately 0.193 hectares.
- 1.2 The site is located within the Harrow Metropolitan Centre and forms part of Opportunity Site 12 in the Harrow and Wealdstone Area Action Plan (AAP). The site is located within 550m of Harrow on the Hill Underground and Train Station and within 200m of the town centre shops and services.
- 1.3 The northern boundary of the site backs onto the rear gardens of the two storey terraced houses on Fairholme Road. The western boundary comprises the side elevation of the two storey semi-detached dwellings on Greenhill Road. On the southern boundary abuts Greenhill Way and the Council owned car park as well as the rear of Debenhams (which forms Opportunity Site 13 in the AAP) is opposite. The eastern boundary is the rear of the Travelodge Hotel, a part 2, 3 and 4 storey building, which forms the remainder of this opportunity site.

2.0 PROPOSAL

- 2.1 The application proposes the redevelopment of the site to provide a total of 89 co-living units (sui generis use) in one building, with three disabled car parking spaces, landscaping, a cycle store for 90 cycles (additional 10 short stay in the forecourt) and internal refuse store.
- 2.2 The building comprises a part 3, part 4, part 5 storey building. The application site is characterised by having a curved front boundary fronting onto Greenhill Way, and as such the proposed building has an obvious three stepped front elevation. Its total width would be 47.9m. The rear elevation is also stepped, although in a much less pronounced manner. The proposed building would have a maximum depth of 24m.
- 2.3 At its nearest locations, the proposed building would be located 1.75m from the western boundary, 6.3m from the northern boundary, 1.75m from the eastern boundary, and 2.15m from the southern boundary.
- 2.4 On the western boundary, the proposed building would be three storeys and 8.9m high. It would then be 7.2m wide to the east and become four storeys with a height of 12m. At this height it would be 9.2m wide to the east before increasing to five storeys and a height of 15m. The five storey element would be 24m wide before decreasing back to four storeys with a maximum height of 12.3m on the eastern boundary. The proposed building would have a flat roof with amenity space included within it.

2.5 The internal configuration is as follows:

Ground floor

- 14 co-living units;
- 24/7 concierge & mail room;
- Kitchen and dining area comprising seating and 8 cooking stations;
- Laundry Facility;
- Residents lounge / co-working;
- Managers Office.

First Floor:

- 26 x co-living units (6 x with front facing and 3 x rear facing balconies);
- Kitchen with 3 cooking stations and linen store.

Second Floor:

- 24 x co-living units (6 x front facing and 3 x rear facing balconies);
- Kitchen with 3 cooking stations and linen store.

Third Floor:

- 20 x co-living units (4 x front facing and 3 x rear facing balconies);
- Study room;
- Outdoor study and garden;
- Linen store.

Fourth Floor:

- 5 x co-living units;
- Gym and outdoor yoga / pilates terrace;
- Games room;
- Private dining / conference room;
- Communal kitchen (1 cooking station);
- Library / lounge;
- Outdoor amenity terrace;
- Linen store.

2.6 Refuse is stored internally and a large cycle store is located to the north east corner of the site. Short stay and accessible cycle spaces are located adjacent to the main entrance of the building at the front. Three blue badge spaces with 2 x electrical charging points are also located to the front of the site. The rear of the site comprises a large communal garden.

3.0 **RELEVANT PLANNING HISTORY**

3.1 A summary of the relevant planning application history is set out in the table below:

Ref no. Description	Status and date of decision
P/1721/08/CFU Redevelopment of former car park to provide block of 37 flats with associated parking (resident permit restricted)	Granted 10/03/2009
P/1414/10 Non-material amendment application for increased number of car parking spaces to planning permission P/1721/08/CFU dated 10.3.2009 For redevelopment of former car park to provide block of 37 flats with associated parking (resident permit restricted) Refused: 12/07/2010	Refused: 12/07/2010
<p><u>Reasons for Refusal:</u></p> <p>1. The proposed amendments would significantly change the design and character of the approved development, would result in an excessive level of site coverage by hard surfaces, would result in a loss of available amenity space and would increase the number of vehicular movements to and within the site. These amendments are considered to be material and would need to be assessed against the requirements of policies 4B.1 and 4B.8 of the London Plan (2008) and saved policies D4, D5, D9, T6 and T13 of the Harrow Unitary Development Plan (2004).</p> <p>2. The proposal is considered unacceptable as a non-material amendment to the Planning Permission Ref; P/1721/08/CFU under Section 190 of The Planning Act 2008 and Section 98A of the Town and Country Planning Act 1990, as amended</p>	
P/3406/10 Modify section 106 agreement to planning permission P/1721/08/CFU dated 10/03/2009 to change proportion of affordable housing at the proposed development	Granted: 10/06/2011
P/2189/11 Extension of time to planning permission P/1721/08/CFU dated 10/03/2009 for redevelopment of former car park to provide block of 37 flats with associated parking (resident permit restricted)	Refused: 08/02/2012
<p><u>Reason for Refusal:</u></p> <p>1. The proposal would provide cramped and substandard accommodation, to the detriment of the residential amenities of future occupiers of the development, contrary to PPS3, policy 3.5 of The London Plan, saved policy D4 of the Harrow Unitary Development Plan (2004) and Supplementary Planning Document: Residential Design Guide (2010).</p>	

<p>P/1653/13 Redevelopment of car park to provide a three four and five storey building to provide 43 flats with associated landscaping and parking</p>	<p>Refused: 26/09/2013 Appeal Dismissed, ref: APP/M5450/A/13/2209903</p>
<p><u>Reasons for Refusal:</u></p> <p>1. The proposal, by reason of its excessive bulk, mass, scale and unacceptable design, would not respect the transitional character of the area and would fail to achieve the design aspirations of the development plan, to the detriment of the character and appearance of the area, contrary to policy 7.4 of The London Plan (2011), policy CS1 of The Harrow Core Strategy (2012), policy DM1 of the Harrow Development Management Policies Local Plan (2013), policies AAP1, AAP2, AAP4 and AAP6 of the Harrow And Wealdstone Area Action Plan (2013) and the adopted Supplementary Planning Document: Residential Design Guide (2010).</p> <p>2. The proposal, by reason of its siting, mass, bulk and proximity to the rear boundaries of Nos. 29-41 Fairholme Road and from the side boundary of No. 59 Greenhill Road, would be visually dominant, overbearing and create a sense of enclosure and result in perceived overlooking from the upper levels, to the detriment of the residential amenities of the occupiers of those properties, contrary to policies 7.4 and 7.6 of The London Plan (2011), policy CS1 of The Harrow Core Strategy (2012), policy DM1 of the Harrow Development Management Policies Local Plan (2013) and the adopted Supplementary Planning Document: Residential Design Guide (2010).</p>	
<p>P/1654/13 Redevelopment of car park to provide a three, four and five storey building to provide 42 flats with associated landscaping and parking</p>	<p>Refused: 26/09/2013 Appeal Dismissed: Ref: APP/M5450/A/13/22009948</p>
<p><u>Reasons for Refusal:</u></p> <p>1. The proposal, by reason of its excessive bulk, mass, scale and unacceptable design, would not respect the transitional character of the area and would fail to achieve the design aspirations of the development plan, to the detriment of the character and appearance of the area, contrary to policy 7.4 of The London Plan (2011), policy CS1 of The Harrow Core Strategy (2012), policy DM1 of the Harrow Development Management Policies Local Plan (2013), policies AAP1, AAP2, AAP4 and AAP6 of the Harrow And Wealdstone Area Action Plan (2013) and the adopted Supplementary Planning Document: Residential Design Guide (2010).</p> <p>2. The proposal, by reason of its siting, mass, bulk and proximity to the rear boundaries of Nos. 29-41 Fairholme Road and from the side boundary of No. 59 Greenhill Road, would be visually dominant, overbearing and create a sense of enclosure and result in perceived overlooking from the upper levels, to the detriment of the residential amenities of the occupiers of those properties, contrary to policies 7.4 and 7.6 of The London Plan (2011), policy CS1 of The Harrow Core Strategy (2012), policy DM1 of the Harrow Development Management Policies Local Plan (2013) and the adopted Supplementary Planning Document: Residential Design Guide (2010).</p>	
<p>Appeal Conclusions (Ref: APP/M5450/A/13/22009948) It is noted that both refused applications P/1653/13 and P/1654/13 were considered and dismissed under the same appeal above.</p>	

In dismissing the above appeals, the Inspector considered that the two main considerations across both cases were;

- The effect of the development on the character and appearance of the area; and
- The effect of the development on the living conditions of the neighbouring residents, with particular regard to outlook.

The inspector concluded that both of the schemes subject to the appeal would, as a result of the height and bulk of the buildings, fail to relate to the domestic scale of the neighbouring buildings and would visually dominate them. The design rationales of each of the two developments would not satisfactorily reduce the bulk or scale of the buildings proposed. The inspector notes that whilst there are steps within the profile of the proposals, they would be minimal and would not be significantly lower than the neighbouring property. It is for these reasons that the Inspector dismissed both appeals.

With regard to the neighbouring occupiers amenity, the Inspector notes that each of the developments would be 7m from the common boundary with the properties fronting onto Fairholme Road. Furthermore, the four storey element would be 25m away for the rear elevations, and the fifth storey some 27m away. The Inspector concludes that a development of a five storey building would be no more significant in terms of being overbearing, or creating a sense of enclosure than would a three or four storey one. With regard to the two appeals, the inspector found no reason to dismiss either of them on grounds of neighbouring residential amenities.

P/5545/15 Redevelopment of car park to provide a three, four and five storey building to provide 42 flats with associated landscaping and parking	Granted 07/06/2017
P/0630/18 Use of part of car park for hand car wash	Granted 07/09/2020
P/3715/19 Redevelopment to create a part 3, part 4 and part 5 storey building with basement to provide 89 co-living units with communal facilities; landscaping; parking; refuse and cycle storage	Withdrawn

4.0 CONSULTATION

- 4.1 Two Site Notices were erected on 07 May 2020 expiring on 03 June 2020.
- 4.2 Press Notice was advertised in the Harrow Times on the 14 May 2020 expiring on 10.06.2019.
- 4.3 The application was advertised as a major application.
- 4.4 A total of 251 notification letters were sent to nearby properties regarding this application. The public consultation period expired on 03 June 2020. A set of revised drawings were submitted to the Council 10 September 2020. The following changes were not considered to be significant enough to warrant a re-consultation:

- Servicing arrangements have been rationalised and located internally;
- 1 x additional blue badge parking space;
- 2 x servicing spaces for waste collection and deliveries;
- Rationalised cycle store to the rear;
- Hedging and privacy screening for ground floor units;
- Building insets at the rear and front reinstated as per approved (P/5545/15);
- Omission of 3 x co-living units at ground floor to accommodate refuse store and relocation of these on upper floors by reducing size of existing units;
- Glass balustrades reinstated as per approved (P/5545/15).

4.5 Adjoining Properties

Number of letters Sent	251
Number of Responses Received	3
Number in Support	0
Number of Objections	3
Number of other Representations (neither objecting or supporting)	0

4.6 3 objections were received from adjoining residents.

4.7 A summary of the responses received along with the Officer comments are set out below:

Social-Economic

Accommodation is for transient people contrary to the need of the Borough which requires long-term accommodation. This could cause overcrowding in Greenhill Road.

Officer Comments:

The application site is located within the Harrow Metropolitan Town Centre comprising a varied mix of town centre uses and as such the intensification of uses in this sustainable location is considered to be appropriate. The type of accommodation proposed is also considered to contribute to the mix of communities in the locality.

Character & Appearance

- Overcrowding has adversely affected local services, already adversely affecting the health and wellbeing of local residents. Local GPs, Northwick Park Hospital (A+E) cannot cope. Adding nearly 100 transient people will exacerbate the situation.
- Will affect visual amenity and surrounding views from No. 55A Greenhill Road.
- Multi-storey building would encroach on the skyline of Debenhams affecting the outlook of No. 14A Welldon Crescent.
- Entrance via Greenhill Road would attract more visitors/vehicles/service vehicles leading to greater parking problems and blocking access to emergency vehicles.
- Development would diminish outlook.

Officers Comments:

Planning permission was previously granted for 42 new dwellings on the site. The revised proposal would therefore not materially increase the associated strain on social infrastructure provisions beyond the scheme that was previously approved. The building proposed is not substantially larger in scale or size than the approved scheme.

Waste Management

Council should not allow a large vehicle to reverse onto Greenhill Road on collection day. This would fail to comply with the general / safety requirements for refuse adopted by the Council.

Excessive noise is generated from large vehicles reversing;

The site should use Greenhill Way entrance for refuse collection like other Greenhill Way residents/commercial buildings.

Officer comments:

Planning permission was previously granted for the collection of refuse from Greenhill Road. The Waste and Highways Officers at the Council have confirmed that the proposed servicing arrangement would be acceptable.

Residential Amenity

Properties on Welldon Crescent would be further overlooked.

The development would be noisy, dusty and privacy would be compromised from Fairholme Road.

Officer comments:

Welldon Crescent is located to the north west of the site, there are no rear facing windows which face the rear gardens of these properties and the corner of the development would be located 9.5m away from their rear boundaries.

The nearest part of the development would be 6.3m from the rear boundaries of Fairholme Road and would be three storeys in height and would be located a further 14m away from the original rear elevations of these properties. This distance has already been accepted as part of planning permission P/5545/15.

Traffic and Parking

- Entrance via Greenhill Road would attract more visitors/vehicles/service vehicles leading to greater parking problems and blocking access to emergency vehicles.

Officer comments:

The servicing of the facility would be as per the existing arrangements along Greenhill Road and would not exacerbate the existing situation. There is a dedicated service and delivery area located in front of the development off Greenhill Road and therefore it is unlikely that vehicles would obstruct traffic flow along the road.

4.8 Statutory and Non Statutory Consultation

4.9 The following consultations have been undertaken:

LBH Environmental Health

No objections but conditions and informatives recommended.

LBH Housing Officer

Avison Young's final report concludes that the scheme will run at a loss (although not as big a loss as the applicant reported). Avison Young noted that the scheme is very sensitive to cost and value movement, so Housing would want to see a late stage review mechanism incorporated into the s106 to try to pick up any uplift.

LBH Drainage

We can confirm that the FRA and Drainage Strategy submitted is satisfactory, however the following details are still required and can be conditioned.

- The applicant should submit a detailed drainage design in line with our standard requirements attached.
- The applicant should consult Thames Water developer services by email: developer.services@thameswater.co.uk or by phone: 0800 009 3921 or on -Thames Water website www.developerservices.co.uk regarding capacity of their public sewers for receiving discharge from the proposed development. ---The Thames Water confirmation letter should be submitted.
- Please request the applicant to submit a cross section of permeable paving construction with full details and their maintenance plan for our approval.

Please be informed that the requested details can be conditioned, attached are our standard drainage conditions/ informative for reference.

Should you need any further clarification, please contact us directly. However, if the applicant requires any advice regarding above, please be informed that our service is chargeable now at £101 per hour and they should contact us via infrastructure@harrow.gov.uk

LBH Highways

The principle of a proposal for a car free development on this site is generally acceptable in highways terms, however further details required such as the location for the additional disabled parking bay that is required from the outset. In order to consider this application further the parking details, swept path drawing for a goods vehicle and a proper Active Travel Zone assessment should be provided.

The above was provided and the Highways Officer raises no further objection.

LBH Travel Plan Officer

Full Travel Plan should include:

Cycling – Links to the Council’s website that promote local cycle training and cycling groups should be noted offer cycle training to residents who would like to learn: <https://www.harrow.gov.uk/road-maintenance-travel/cycling-harrow?documentId=12445&categoryId=210280>.

6.3.2 – Targets to be reviewed following the baseline survey and not in year 1. This needs to be updated as targets are quite ambitious and will need to be reviewed.

Measures/ Initiatives to be revisited following baseline survey. This needs to be included within a full Travel Plan.

7 – The TPC should be in place 2 months prior to development and Travel Planning team to be informed of contact details – Travelplanning@harrow.gov.uk

Action plan – Please add action to have TPC in place 2 months prior to occupation

Travel Plan monitoring

Please include a Monitoring section with details of how the plan will be monitored, including:

- Baseline Survey to be completed within 6 months of agreement (exact options to be secured by agreement) and following surveys to be conducted in year 1, 3 and 5 following the baseline survey.
- All financial costs associated with the Travel Plan are to be provided by the developer
- Travel Plan is to be secured by agreement
- Travel Plan Monitoring fees/bond to be secured with the latter only to be used in the event of the development not meeting targets in year 5

LBH Planning Policy

The proposed development would provide housing choice in a highly sustainable location of Harrow, being within the Harrow Metropolitan Town Centre. Whilst there is no demonstrable need for such a typology, demonstrating its need is not a requirement within the policy which supports such a typology. In the event that the proposed development is able to provide financial contribution in lieu of affordable housing being provided on site, planning policy would not be in a position to object to the scheme. Such a contribution would assist in meeting an identified need insofar as providing for affordable housing which can be of a family size.

Affordable Housing

Draft New London Plan (2019)(Intend to Publish Version) requires that large-scale purpose built co-living accommodation provide a contribution toward affordable housing. However, given that the proposed accommodation typology does not require to meet national minimum space standards, any

contribution would not be made on-site, instead would be by way of a financial contribution. This would be secured by way of a S.106 obligation. It is noted at this stage that the submitted financial viability assessment submitted in support of the application, states that the proposed scheme is unable to provide any contribution to affordable housing. However, the applicant has indicated that they may be in a position to offer a financial contribution notwithstanding the outcomes of the financial viability assessment. At this stage the offer is unknown and would be subject to further interrogation and review.

LBH Energy and Sustainability Officer

Energy and Sustainability Statement (Integration, May 2020) generally reflects that requirements of the current and 'Intend to Publish' London Plan in that a minimum 35% carbon reduction is proposed on site (35.9%) with the balance carbon emissions (residential element – 36.48 tonnes) to be offset through monetary contribution (£65,672) to achieve the required zero carbon residential development.

Within the 35.9% reduction on-site, 9.7% will be through energy efficiency measures (residential element), broadly meeting the proposed 10% in the 'Intend to Publish' London Plan. The energy efficiency savings from the non-residential element is 6.0%, which is below the 15% requirement of the 'Intend to Publish' London Plan and is disappointing, but this only forms a relatively small part of the scheme.

The statement correctly identifies that there are no district heat networks capable of serving the site, but the provision of an on-site communal heating network will enable future connection should a district network become available; this is acceptable.

A 26.2% reduction is achieved through solar PV, which is reasonable given constraints with respect to roof space.

Should the application be granted, the following condition should be required:

- Implementation of the development generally in accordance with the Energy and Sustainability Statement, with at least a 35% reduction in carbon emissions on-site for residential and non-residential elements of the development.
- S106 planning obligations:
 - Safeguarding a route for future connection to a district heat network
 - Requiring payment of the carbon offset contribution (£65,672) prior to commencement of development
 - Post-construction assessment to determine if the proposed carbon reductions have been achieved and whether an additional carbon offset contribution (residential element) is required to offset any shortfall, calculated at £1,800 per tonne of carbon.

LBH Urban Design Officer

- Concern with how delivery and refuse vehicles will park beside site and their use of reverse gear;
- The refuse store is a standalone block which is a poor addition to the landscaping amenity space. Additionally, refuse spaces are doubled with the provision of a holding area. These two spaces should be rationalised and ideally incorporated within the building envelope itself to create additional outdoor amenity space.
- Generally, there is adequate planting shown on plans however minimal detail has been provided with regard to a planting strategy.
- It is unclear why there are such extensive enclosed and private elements to landscaping given that these do not form part of ground floor private amenity spaces. If they are to be accessed and used by residents, this will negate their purpose as privacy screens.
- External cycle stores should be covered and enclosed for adequate security and weather protection.
- Concerns with kitchen size per floor and whether this is adequate, as well as the lack of lounge provision on each floor.
- The kitchen dining space presents a utilitarian layout with little celebration of communal cooking and dining it is also unclear where refrigerator provision is located, however double door aspects to an outdoor dining area is welcome.
- Likewise, the ground floor lounge is dense with seating and does not allow space for sufficient relaxation. Tables should be reduced in favour of softer furnishings. It is also unclear what the space adjacent to the kitchen and dining room is intended for.
- Almost all units are single aspect and while some units benefit from private balconies this does not overcome the poor and highly limited amount of space and natural light each unit offers, particularly for north-facing units.
- Proposed external materials are of a high quality, including proposed brick, window and door reveals.
- Given the varied and potentially transitory nature of residents, glazed balustrades are not suitable and a high-quality metalwork balustrade which is complimentary to door and window reveals is highly encouraged.

LBH Landscape Architects

No objection subject to appropriate landscape, boundary +
-
treatment, external lighting conditions.

LBH Waste Officer

Allocation for 22 x 1100 bins would need to be provided. The appropriate pulling distance needs to be provided.

The plans have been revised and the Waste Officer has no further objections.

Biodiversity

No objections subject to pre-commencement conditions.

Economic Development Officer

Construction Training

As a major application, Economic Development will be seeking construction employment opportunities on site in line with Policy E11 Skills and Opportunities For All of the London Plan.

This will be secured through:

- (i) a training and employment plan that will be agreed between the council and the developer prior to start on site and
- (ii) a financial contribution towards the management and delivery of the construction training programme based on the build cost of the development. This is usually charged based on a formula of £2500 per £1m build cost.

Network Management

The dropped kerbs for the bin stores should be indicated on the drawing, I have no concerns regarding the 2 disabled bays accessed via Greenhill Road and the temporary access to be created on Greenhill Way and for them both to be returned to raised footway on completion of the build.

- 4.10 The comments of the consultees are addressed within the relevant sections of the assessment.

External Consultation

- 4.11 A summary of the consultation responses received are set out in the table below.

Designing Out Crime Officer

- The development requires a high quality of access control, with full compartmentation, data logging and destination control lifts.
- Good quality CCTV is required.
- Doors must meet minimum door specifications
- All ground floor windows should be tested and certified to PAS24081; 2016 with BS EN 356:2000 Class P2A glazing.
- All external gates to be minimum tested and certified.
- Lighting will need to conform with BS5 489:2013 standard for all external doors, windows, gates, refuse and cycle stores.
- A strip of defensible planning will be required in front of residential windows.

If planning permission is to be granted it is strongly recommended that Secure by Design accreditation is secured by way of planning condition.

5.0 POLICIES

5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that:

'If regard is to be had to the Development Plan for the purpose of any determination to be made under the Planning Acts, the determination must be made in accordance with the Plan unless material considerations indicate otherwise.'

5.2 The National Planning Policy Framework (NPPF) sets out the Government's planning policies for England and how these should be applied; it is a material consideration in the determination of this application. The current NPPF was published in July 2018 and was updated in February 2019.

5.3 In this instance, the Development Plan comprises The London Plan 2016, The Harrow Core Strategy 2012 [CS], Harrow and Wealdstone Area Action Plan (AAP) 2013, the Development Management Policies Local Plan 2013 [DMP], the Site Allocations Local Plan SALP 2013 [SALP].

5.4 While this application has been principally considered against the adopted London Plan (2016) policies, some regard has also been given to relevant policies in the Draft London Plan (2017), as this will eventually replace the current London Plan (2016) when adopted and forms part of the development plan for the Borough.

5.5 The document was originally published in draft form in December 2017 and subject to Examination in Public (EiP) with the Panel's report published in October 2019. The Mayor of London has considered these recommendations, and has either accepted them or where not, provided justification as to why accepting them would not be appropriate. The Mayor has now submitted to the Secretary of State an 'Intend to Publish' version of The Plan. It is for the Secretary of State to determine whether he agrees with the revised Plan and it ought to be published in that form.

5.6 The Draft London Plan is a material planning consideration that holds significant weight in determining planning applications, with relevant policies referenced within the report below.

6.0 ASSESSMENT

6.1 The main issues are;

- Principle of the Development
- Design, Character and Appearance of the Area
- Residential Amenity
- Affordable Housing
- Traffic, Safety & Parking
- Development and Flood Risk
- Trees, Ecology and Biodiversity
- Sustainability and Climate Change Mitigation
- Land Contamination and Remediation
- Air Quality

- Statement of Community Engagement
- S.106 Obligations and Infrastructure

6.2 Principle of Development

6.2.1 The relevant policies are:

- National Planning Policy Framework (2019)
- The London Plan (2016): 2.15, 3.3, 3.5, 3.8
- The Draft New London Plan (2019); D1, H1, H16
- Harrow Core Strategy (2012): CS1
- Harrow & Wealdstone Area Action Plan (2013): AAP13 and Allocated Site12
- Harrow Development Management Policies Local Plan (2013): DM1

6.2.2 Policy 3.3 of the London Plan seeks to increase housing supply in order to promote opportunity and provide a real choice for all Londoners in ways that meet their needs at a price they can afford. Core Policy CS1(I) states that ‘new residential development shall result in a mix of housing in terms of type, size and tenure across the Borough and within neighbourhoods, to promote housing choice, meet local needs and to maintain mixed and sustainable communities’

Delivery of Site Allocation Uses

6.2.3 The North Side Car Park on Greenhill Way is an allocated site (Site 12) within the Harrow & Wealdstone Area Action Plan (2013). The site includes the car park to the west of the site (the application site) and part of the site to the east of the application site which has been redeveloped as a Travelodge Hotel. The site allocation seeks two leading land uses on the site, comprising C3 residential and C1 hotel. It is noted that the proposed use (when assessed strictly in accordance with draft London Plan Policy H16), would not be a C3 residential use, rather it would represent a Sui Generis use. The site allocation does not allow for uses outside of the C1 or C3 use class. However, a consideration of this use on the site is provided below.

6.2.4 The commentary notes among other matters included as part of the Site Allocation, states that the allocated site is earmarked to provide for a target of 35 homes. Furthermore, any building should be between 3 – 4 storey’s in height and address the curvature of the road at this location. In 2009 the Council resolved to grant planning permission (subject to a legal agreement) for the redevelopment of the former car park to provide block of 37 flats with associated parking (resident permit restricted) under application P/1721/08/CFU. This scheme was never implemented and has now lapsed. Since then planning permission was granted for 42 homes under planning permission P/5545/15.

Acceptability of current proposal

6.2.5 The proposed development seeks planning permission for 89 co-living units (sui generis use). To this end, the supporting planning statement states that the proposal would contribute towards the Borough’s housing targets, however given that the 89 co-living units do not correlate directly into 89 net additional units it is considered that this notion is not strictly true. Indeed, Policy H1 of the Draft New London Plan “Intend to Publish version” (2019) (Para 4.1.9) states that non self-contained communal accommodation should count towards meeting the housing

targets but only based on a 1.8:1 ratio. Therefore, the proposal would have a net contribution towards the borough's housing target of circa 49 net additional units. On this basis, although not strictly C3 use, the co-living units would contribute to the Borough's housing target and would therefore be an appropriate use as per the site allocation.

6.2.6 The layout and management of co-living accommodation is unlike conventional housing and therefore does not fall within any Use Class (i.e. sui generis). It is noted that there is no national or local policy in relation to this typology, so all assessment of the acceptability of the proposal therefore falls under the Draft New London Plan (2019) (Intend to Publish Version), specifically by way of policy H16 (Large-scale purpose-built shared living). H16 is a criteria-based policy, which sets out the requirements that any proposed co-living development must comply with. This policy provides support for such a use provided, that it meets the criteria set out within it. The compliance with these criteria in terms of the accommodation and ancillary uses are considered below.

6.2.7 Policy H16 states that large scale purpose built shared living development should meet the following criteria:

- 1) It is of good quality and design;
- 2) It contributes towards mixed and inclusive neighbourhoods;
- 3) It is located in an area well-connected to local services and employment;
- 4) It is under single management;
- 5) Its units are all for rent with minimum tenancy lengths of no less than 3 Months;
- 6) Communal facilities and services are provided that are sufficient to meet the requirements of the intended number of residents and offer at least:
 - Convenient access to a communal kitchen
 - Outside communal amenity space
 - Internal communal amenity space
 - Laundry and drying facilities
 - Concierge
 - Bedding and linen changing/room cleaning services
- 7) The private units provide adequate functional living space and layout; and are not self-contained homes or capable of being used as self-contained homes;
- 8) A Management Plan is provided; and
- 9) It delivers a cash in lieu contribution towards conventional C3 affordable housing. Boroughs should seek this contribution for the provision of new C3 off-site affordable housing as either an:
 - a) upfront cash in lieu payment to the local authority, or
 - b) in perpetuity annual payment to the local authority

6.2.8 The applicants have developed a number of co-living schemes including in Wembley for 368 units and 97 units in Hounslow. As per these developments, they have confirmed that they would be the sole managers of the proposed development. The submission is accompanied by a Management and Operation Plan. This provides further detail on the management of the building including the

role of the onsite building management team, tenancy, co-living accesses and facilities.

- 6.2.9 Officers consider that the proposed development would be a high quality scheme that would raise the standard of architecture in the area (detailed further in section 6.3 of the report). The proposed housing product would contribute towards a mixed community and is located within the Harrow town centre boundary and therefore is in close proximity to sustainable transport modes. Furthermore, the proposed scheme would provide a satisfactory quantum of internal and external amenity space and communal facilities for the future occupiers and the minimum tenancy lengths and management plan are to be secured through the Section 106 agreement. Officers consider that the proposed co-living accommodation would therefore satisfy the requirements as set out in policy H16 of the draft London Plan (2019).
- 6.2.10 The proposed development would provide housing choice in a highly sustainable location of Harrow, being within the Harrow Metropolitan Town Centre. Whilst there is no current demonstrable need for this housing typology within the Borough, the proposed co-living accommodation would meet an emerging need as supported within the draft London Plan and a planning obligation would ensure the development will be managed and maintained in a manner which would ensure the continued quality of the accommodation. The proposal would make a noteworthy contribution towards the wider policy and regeneration objectives for the heart of Harrow, including housing and employment. Having regard to compliance with these elements of the development plan, together with other considerations in this report with respect to design, amenity, infrastructure, servicing and highways; the principle of the development is supported by officers.

Affordable Housing

- 6.2.11 The Draft New London Plan (2019) (Intend to Publish Version) requires that large-scale purpose built co-living accommodation provide a contribution toward affordable housing. However, given that the proposed accommodation typology is not required to meet national minimum space standards, any contribution would not be made on-site, instead would be by way of a financial contribution. This would be secured by way of a S.106 obligation. It is noted at this stage that the submitted financial viability assessment submitted in support of the application, states that the proposed scheme is unable to provide any contribution to affordable housing.
- 6.2.12 Based on the above, the proposed development complies with the National Planning Policy Framework (2019), policy 2.15, 3.3, 3.5 and 3.8 of The London Plan (2016), policy SD6 and H16 of The Draft London Plan (2019), policies AAP13 of the Harrow and Wealdstone Area Action Plan (2013), policies CS1 of the Core Strategy (2012) and policy DM1 of the Development Management Policies Local Plan (2013).

6.3 Design, Character and Appearance of the Area

6.3.1 The relevant policies are:

- National Planning Policy Framework (2019)
- The London Plan 2016: 3.3, 3.5, 3.8, 6.3, 6.9, 6.13, 7.4, 7.6
- The Draft London Plan 2019: D1, D2,
- Harrow Core Strategy 2012: CS1
- Harrow Development Management Policies Local Plan (2013): DM1, DM9, DM10, DM22, DM23, DM27, DM42, DM44, and DM45

Relevant Supplementary Documents

- Residential Design Guide (2010)
- The London Plan Housing Supplementary Planning Guidance (2016)
- Code of Practice for Storage and Collection of Refuse and Materials for Recycling in Domestic Properties (2016)

6.3.2 The application site is currently vacant and sits on the northern side of Greenhill Way. At this location, Greenhill Way has a pronounced curve in the highway, as it curves away from a northerly direction to the east. As mentioned above, any future development within the application site would need to address this curve within the highway, to ensure an appropriate relationship between the two.

6.3.3 Two previous applications (P/1653/13 and P/1654/13) at the subject site were for similar schemes were refused and subsequently dismissed at appeal (ref: APP/M5450/A/13/22009948). Subsequently, planning permission was granted (P/5545/15) for 42 residential properties at the site, and the revised design was considered to overcome previous concerns raised by the Inspector in relation to the design and character. This approved design was subsequently shortlisted for innovative architectural design at the Housing Design Awards in 2016. The current scheme, has therefore been designed using the previous approval (P/5545/15) as a starting point and with the agreement of the architects (Allied Godwin Developments Limited) who designed it. Crucially, the stepped form is retained to the front elevation and at roof level, providing clear and purposeful steps which allows the building to maximise the width of the property whilst ensuring that it does not present an overly dominant, blank wall adjacent to the public highway.

6.3.4 Whilst the footprint, scale and mass largely replicate that approved, there are some key differences. The main differences include a single storey reception area to the front elevation as well as a front facing conservatory at fifth floor level. Notably there are also changes to the outdoor amenity spaces and architectural details on the facades to suit the co-living arrangements. The general layout of the site frontage has also changed primarily in relation to the delivery, servicing arrangements, car parking (to include three blue badge spaces) and landscaping. These and other minor changes are outlined in a separate document within the submission (*Comparison: Proposed scheme versus approved residential scheme, September 2020*).

- 6.3.5 Given that these changes are considered minor in the context of the development and would not compromise the intentional design form which was considered acceptable in design and amenity terms in the approved scheme, it is considered that the proposal would remain complementary to the streets scene, be of an appropriate scale, design and mass and would therefore be acceptable. The proposed breaks within the front elevation and the steps within the roof profile are minimal in number and purposeful, which ensure that the bulk of the proposed building is satisfactorily broken up, without becoming overly fussy. The footprint and profile of the proposed building ensure that appropriate articulation is provided and that the curvature of the public highway has been addressed.
- 6.3.6 In terms of relationship with the street scene and the surrounding area, at the western end of the site, the proposed building would be three storeys high, with a flat roof, adjacent to the two-storey dwelling at No. 59 Greenhill Road. Although three storeys high, it is noted as being situated lower than the roof ridge of No. 59 Greenhill Road. The proposed building would then make purposeful steps up to four storeys, then to a total of five storeys across the central part of the building when read from Greenhill Way. Where the proposed building is located on the eastern boundary, adjacent to the three storey hotel, it would be four storeys in height. The proposed steps within the roof profile are purposeful and ensure that a linear and continuous roof profile is not created within the streetscene. To the rear, the building would remain as per the approved scheme and as such is considered to have an appropriate relationship with the adjoining properties on Fairholme Road and Welldon Crescent.
- 6.3.7 The Council's Design Officer notes that in terms of materiality the proposed external materials are of high quality, including the mix of brickwork and cladding, window and door reveals to provide architectural interest. It is however noted that the glazed balustrades are not considered appropriate and suggested that high quality metal work balustrades would be more complimentary. To ensure that high quality materials, windows, frames and drainage pipes are used, a condition requiring details to be submitted to the LPA for review is recommended. This will also include details of the proposed window reveals to enhance and safeguard the character and appearance of the area.

Access

- 6.3.8 The proposed development would have a single main entrance into the building from Greenhill Way. It would be identifiable as it would offer a break from the consistent brickwork on the façade and present a fully glazed entrance. A lift is proposed to enable inclusive access to all floors within the development. An access way is also proposed along the western boundary between the new build and No. 59 Greenhill Road. This would provide access to the rear communal garden and secure bicycle store. Similarly, a side access is provided on the eastern side. Parking and other traffic related matters are assessed under the requisite section in this report.

Landscaping

- 6.3.9 The communal external amenity spaces have been well designed and there are a number of different types of external, landscaped spaces proposed by way of ground floor communal gardens as well as roof terraces as well as some units benefitting from private balconies. The plans have been amended to internalise

the refuse store and rationalise the cycle storage to the rear to maximise soft landscaping and offer of useable outdoor space. The Council's Landscape Officer was consulted on the proposal, and raised no objection to the proposed development, subject to conditions in relation to the submission of a landscape masterplan, including soft and hard landscaping, a landscape management and maintenance plan, details of the boundary treatments, levels and external lighting. The appropriate conditions have been added accordingly to ensure that it delivers a space of quality that can be used and appreciated by residents.

Refuse and Servicing

- 6.3.10 Policy DM45 of the Development Management Policies Local Plan states that 'all proposals will be required to make on-site provision for general waste, the separation of recyclable materials and the collection of organic material for composting'. In terms of character and appearance, this policy requires refuse storage bins to 'be located and screened to avoid nuisance to occupiers and adverse visual impact'.
- 6.3.11 The development would provide for space for 24 x bins within a dedicated internal store accessed by service vehicles from Greenhill Road (north western end of the site). This would allow for appropriate servicing distances. As there is not sufficient space at the end of Greenhill Road for the refuse vehicle to turn, a reverse manoeuvre is required. This is consistent with the strategy approved under the consented scheme and with the existing collection regime serving properties along Greenhill Road. The Council's Waste Officer has stated that the number of bins, siting and location is acceptable. A condition ensuring that refuse bins are always stored within the internal refuse area is recommended to safeguard the character and appearance as well as the residential amenity of the area.
- 6.3.12 It is therefore considered that the proposed development would not result in a detrimental impact to the of the surrounding area, in accordance with the high quality design character and appearance aspirations of the National Planning Policy Framework (2019), policies 3.5, 7.4 and 7.6 of The London Plan (2016), policy CS1 of the Core Strategy (2012), policies DM1, DM22 and DM23 of the Harrow Development Management Policies Local Plan (2013) and Area Action Plan (2013) policies AAP1, AAP4, AAP6 and AAP13 the Supplementary Planning Document: Residential Design Guide (2010).

6.4 Residential Amenity

- 6.4.1 The relevant policies are:
- National Planning Policy Framework (2019);
 - The London Plan (2016): 7.6
 - The Draft London Plan: D1, D6, D14, H16
 - Harrow Core Strategy: CS1
 - Harrow Development Management Policies Local Plan (2013): DM1, DM27

Relevant Supplementary Documents:

- Residential Design Guide (SPD);
- The London Plan Housing Supplementary Planning Guidance (2016)

- 6.4.2 There are no specific adopted space standards for this type of housing product within the adopted development plan. Policy H16 of the draft London Plan (showing minor suggested changes) states that the private units within large-scale purpose-built shared living development should provide adequate functional layout.
- 6.4.3 Each of the proposed co-living units would have on average 18-26sqm of internal space and a minimum floor to ceiling height of 2.5m. The internal layout has been designed to accommodate a lift bed, pull out bar, small kitchenette, built in storage and a shower room (see image below). Some units also benefit from private balconies which would either face Greenhill Way towards the Harrow Town Centre to the front or towards the communal garden to the rear or rear gardens of Welldone Crescent or Fairholme Road which are sufficiently distanced away from the development.



- 6.4.4 All units would be single aspect with either a north-westerly or south and south-easterly orientation. The Daylight and Sunlight Report provided in support of the application concludes that the majority of units would achieve good levels of daylight and sunlight which would be in excess of the BRE and British Standard targets. In terms of Affecting Daylight Factor (ADF) all units would achieve above targets for bedrooms. In terms of sunlight, 53 of the 89 co-living units would achieve the recommended level of 25% total and 5% winter sunlight. Whilst not ideal, given a large number of units fall below the BRE target levels, there are three alternative communal areas that achieve good levels of sunlight. As the co-living facility encourages communal living, it is considered on balance that there are sufficient mitigating factors within the design to overcome the shortfall of the scheme in this regard. No windows are proposed on the flank elevations, other than a small obscurely glazed window facing the Travel Lodge Hotel which would provide some light into the communal hallway on the upper floor levels.
- 6.4.5 In terms of stacking, the 12 units at first floor level would be sited above the ground floor co-working/café and communal areas. Six units at 2nd floor level would be sited beneath the third floor roof terrace. Furthermore, the majority of the third floor units would be sited beneath the fourth floor communal areas. The proposed building is a purpose-built new building, which would need to comply with building regulations with regards to sound insulation measures to ensure there would be no unacceptable noise transmission. Therefore, the proposed development is acceptable in this regard.
- 6.4.6 Criteria 6 of the Draft London Plan Policy H16 requires that communal facilities are sufficient to meet the requirements of the intended number of residents and offer a range of internal and external communal amenity spaces and facilities. This

ensures that the quality of overall residential amenity is acceptable. A schedule of amenity has been provided as part of the application. Based on this, each occupier would benefit from 3 sqm of internal amenity space and 8.2 sqm of external amenity space. The internal space differs from the 5 sqm stated by the applicant, however the Council do not consider the reception area to contribute towards the internal amenity space and have deducted this from the calculation. Notwithstanding this, it is considered that the appropriately sized rooms (including some with private balconies) and private facilities coupled with the range of amenity spaces available would overcome this marginal shortfall. A breakdown of the proposed internal and external amenity spaces is summarised below:

Internal Amenity	Ground Floor	First Floor	Second Floor	Third Floor	Fourth Floor	Amenity Net. (sqm)
Lounge / Coworking	68					68
Seating	12					12
Lounge / Bar					82	82
Kitchen	67	20	20	20	10	137
Laundry	26					26
Library / Study				14	15	29
Dining Room					20	20
Games Room					12	12
Gym / Wellness					42	42
Sub-Total	173	20	20	34	181	452
External Amenity						
	491			53	184	728
TOTAL	688	20	20	87	365	1180
Private Amenity	No. units	No. units	No. units	No. units	No. units	
	5	8	7	7	0	

6.4.7 As demonstrated above, the proposed development would provide a range of internal and external communal spaces which have been designed to create a sense of community, lower barriers to social interaction and to facilitate engagement between residents. The amenity spaces are large, high quality which encourage their use and community engagement. The provision is therefore considered to be of an appropriate type, location and amount which would be proportionate to the number of studio units within the building.

6.4.8 The site is located within an urban location amongst a mix of residential and commercial uses. Given the transient nature and number of future occupants of the development, the Environmental Health Officer has stated that an Anti-Social Behaviour Action Plan is submitted to minimise disruption and has suggested an informative to this effect is included. Notwithstanding this, to ensure the safety of existing and future residents of the immediate vicinity Secure By Design accreditation should be attained and as such a condition to this effect has been included.

Response to COVID 19

- 6.4.8 Whilst not a policy requirement, the current planning application has been submitted under the context of a global pandemic whereby the health and safety of future occupiers is paramount. Co-living relies upon social interaction and engagement and whilst this is important for mental well-being this needs to be carefully measured against safety guidelines imposed by National Government. The applicant has submitted a document outlining the response to COVID 19 including measures to consistently ensure the level of hygiene in communal areas, installation of air purifiers with filters in each unit, provision of Personal Protective Equipment to name a few. It is therefore considered that the co-living nature of the development would provide a level of flexibility and adaptability during unprecedented times as in the current climate.
- 6.4.10 The applicant has submitted a Fire Strategy by MLM Group who has qualified assessors of fire safety in buildings. The strategy documents include provision of automatic water fire detection systems (AWFSS) throughout the building and within each unit. There are two stair cores to allow for efficient evacuation and all kitchens are positioned centrally to mitigate risk of fire spread and would include sprinkler systems. These measures will be verified at Building Control stage to ensure compliance with Part B of the Building Regulations.
- 6.4.11 In conclusion, the proposed development, subject to the imposition of appropriate conditions would achieve a high-quality internal design quality of the proposed development is of an acceptable standard and will ensure a good standard of living environment for future residents.

Daylight & Sunlight Report

- 6.4.12 The impact of the proposed development on the amenities of neighbouring occupiers is considered in detail below. However, it is noted that the applicant has submitted a Daylight & Sunlight Report in support of the planning application. Specifically, the assessment considers the impact to the nearest properties namely 2 Greenhill Way, 59 Greenhill Road and 27-45 Fairholme Road.

Properties along Fairholme Road

- 6.4.13 The applicant has taken guidance from the previously approved scheme for 42 residential units (P/5455/15) and has largely replicated the footprint, scale and mass of the development. The two additional elements (fourth floor conservatory and ground floor reception) are sited centrally and contained within the recesses of the stepped formation to the front and would therefore not result in loss daylight or sunlight to neighbouring properties over and above the approved scheme. The Daylight & Sunlight Report state that all facing rooms of properties along Fairholme Road would retain levels of daylight in excess of the given targets and would therefore be consistent with the BRE guidance.
- 6.4.14 The proposed development at its closest point, would be 6.1m, and would be three storey's high (9.3m). It would then increase to 12.4m high (4-storey's) and be 9.6m away, and then 15.4m (5-storey's) and 12.8m away. It is considered that the proposed development would be located approximately within the same proximity to the common boundary with the properties fronting onto Fairholme Road, and

would not be higher than the previously approved scheme (total height or at each finished roof level of each floor). Accordingly, it is considered that the proposed development would not result in a loss of light or outlook to the properties, namely 29 – 41, that front onto Fairholme Road to the north of the application site.

- 6.4.15 It is noted that there a number of communal areas by way of roof terraces at third floor level and above that are proposed. Whilst their siting and size largely replicate that approved by planning application P/5455/15, care has been taken to demonstrate that appropriately positioned screening would be located along the northern edge of the terraces. The submitted plans demonstrate that a relatively deep planter box would be located on the terrace edge, which would be planted with soft landscaping. The proposed depth of the planter box, in conjunction with the proposed soft landscaping, would ensure that future occupiers utilising the roof terrace would not have a clear line of sight down into the rear gardens of the properties fronting Fairholme Road. Subject to a condition ensuring further details of these are provided, and that the soft landscaping is appropriate and maintained long term, this would be considered acceptable. Furthermore, a planning condition has been attached to ensure the on-going management and maintenance of the hard and soft landscaping across the site.
- 6.4.16 The current scheme would continue to have habitable windows and balconies on the rear elevation, which would face the properties fronting onto Fairholme Road. However, it is considered that given the similarities in height and proximity to the previous scheme, which were found to be acceptable in neighbouring amenity terms, the proposed scheme would also be considered acceptable. Private and communal amenity space would be located to the rear of the new building, which would be similar to the previous scheme. It is proposed that a close boarded timber fence be used as boundary treatment along the rear and flank boundaries. The use of the private amenity space is considered to be acceptable, as this would be residential use that would be similar to that as the adjoining properties facing Fairholme Road.
- 6.4.17 It is considered that the proposed development, subject to safeguarding conditions, would have an acceptable impact on the residential amenities of the existing occupiers fronting onto Fairholme Road.

Properties along Greenhill Road

- 6.4.18 The properties located along the northern side of Greenhill Road are three pairs of semi-detached dwellings. No. 59 Greenhill Road has its flank elevation running parallel with the western flank elevation with the application property. It is noted that the current scheme in terms of the relationship with Greenhill Road, in so far as the flank elevation on the common boundary is concerned, is parallel with No. 59 and also sits deeper within the site.
- 6.4.19 The proposed elevation on this boundary would extend beyond the rear elevation of No. 59 Greenhill Road by 4.6m, which would result in the proposed development breaching the relevant horizontal 45 degree plane marginally as detailed within paragraph 4.68 of the Residential Design Guide SPD (2010). This 45 degree plane seeks to protect the amenity of adjoining residential occupiers, by ensuring an appropriate set off and depth of developments adjacent to residential properties. However, when measured from the rear facing windows at No. 59 Greenhill Road it would demonstrate compliance with this rule. As accepted in the

approved scheme compliance with this element assists in ensuring that the proposed development would not result in an unacceptable loss of light, outlook or appear as an overly dominating structure on the common boundary with these occupiers.

- 6.4.20 In terms of sunlight and daylight, the submitted report assessed the impact of the development on all habitable windows. This states that 6 of the 7 windows within the assessment met the associated BRE criteria. One window is noted as falling below the prescribed targets but is identified as being mitigated by a fully compliant window and the room it serves.
- 6.4.21 Whilst a number of communal spaces are located in close proximity to this property, the proposed roof terraces would be set back from the edges of the roof. Furthermore, the proposed plans indicate that landscape planters would be located around the edge of the amenity space (set off the edge of the roof), which would provide a level of screening. Accordingly, the setting in of the amenity space from the edge of the roof, in conjunction with the proposed landscaping would ensure that users of this would not have direct views into the rear of No. 59 Greenhill Road.
- 6.4.21 According to the submitted plans, private balconies that are located to the rear, and on the western end of the proposed new build, would be able to view into the rear of this property. It is therefore considered that privacy screens within the flank of these units is required to ensure that the amenities of the occupiers of No. 59 Greenhill Road would provide neighbouring privacy. Subject to such condition, the proposed development would not result in unacceptable harm to the occupiers of No. 59 Greenhill Road, or indeed residents further along the north side of Greenhill Road, by reason of a loss of privacy, overlooking or perceived overlooking. Specific units requiring privacy screens, would be secured by way of a condition.

2 Greenhill Way

- 6.4.23 This property is located south-west of the proposed development and is a two storey dwellinghouse on the opposite side of Greenhill Road. There are a number of windows within the north facing flank elevations that overlook the proposed development. The Daylight & Sunlight assessment states that all windows would retain levels of daylight in excess of the given targets and would be consistent with the BRE guidance. Furthermore, the property is located over 15m away from the development which would ensure sufficient distance to ensure there would be limited loss of outlook and privacy.

Travelodge

- 6.4.24 The hotel development consists of a part two, part three and part four storey development, with a narrow frontage onto Station Road and extending west along Greenhill Way. At the western end of the proposal, it is three storeys high and has a flat roof. The hotel is located 1.4m off the common boundary with the application site, with the elevation fronting onto Greenhill Way angled towards the application site. Although there are no ground floor side facing habitable windows, the front elevation facing onto Greenhill Way does contain windows. However, as this elevation would be angled in a south-western orientation, with the proposed building within the application site sitting some 4.6m forward of the hotel elevation in this corner there would still be a relatively open southerly aspect to this

elevation, and as such occupiers would receive an adequate level of outlook and light. No habitable windows are located on the flank elevation of the proposed development which would result in overlooking to that side.

- 6.4.25 The western flank elevation of the hotel, when viewed from Greenhill Way, has rooms located in the rear half (towards Fairholme Road) and habitable windows facing out over the application site. The orientation of this elevation is noted as being in a more northerly direction, with direct views being towards the rear boundary. As such, it is considered that there would only be limited direct views into these habitable windows, with most views being oblique. Accordingly, it is considered that the proposal would have an acceptable impact to the hotel rooms in terms of residential amenity.
- 6.4.26 It is proposed to have a terrace on the flat roof of the third floor element which would provide private amenity space for a single unit. However, this would be set in from the neighbouring boundary by approximately 10m and screened with soft landscaping and appropriate privacy screens. At fourth floor level an outdoor terrace associated with the wellness centre / gym is located close to boundary with the hotel. However, it is noted that the amenity space would be set back from the edge of the roof, and would have soft landscaping raised planter boxes on the edge of the amenity area. It is considered that the proposed mitigation measures would ensure that the amenity of occupiers of these rooms would be protected. The rear facing windows would be at an oblique angle and as such would not result in unacceptable harm through overlooking or loss of privacy.

Debenhams Car Park

- 6.4.27 To the south of the site across the road is the car park. It is acknowledged that this site is Council owned and is an allocated site in the Council's adopted AAP. However, there is no formal planning application submitted regarding this site. In any case, its lawful use is as a car park, which it is currently occupied as. The proposed development would have no unacceptable impacts on the current and lawful use of this site.

Conclusion

- 6.4.28 Overall and subject to conditions, the proposed development is considered on balance to be acceptable in terms of the living conditions of neighbouring occupiers, and would meet the policy objectives of the relevant Development Plan policies.

6.5 Affordable Housing

- 6.5.1 The relevant policies are:

- The London Plan 2016: 3.12, 3.13;
- The Draft London Plan (2019): H16;
- Harrow Core Strategy (2012): CS1;
- Harrow Development Management Policies Local Plan (2013):DM1, DM50

Relevant Supplementary Documents:

- Mayors Affordable Housing and Viability Supplementary Planning Guidance (2017)

- Supplementary Planning Document: Planning Obligations and Affordable Housing (2013)

- 6.5.2 Policy 3.12 of the London Plan requires councils to seek the maximum reasonable amount of affordable housing when negotiating on individual private residential and mixed-use schemes. At the local level, overarching Policy CS1J of the Core Strategy (2012) sets a borough-wide affordable housing target of 40% of the housing numbers delivered from all sources of supply across the borough and calls for the maximum reasonable amount to be provided on development sites.
- 6.5.3 The Housing SPG confirms that new large scale shared living housing should contribute the maximum reasonable amount of affordable housing it can, in line with London Plan policies 3.12 and 3.13, but does not set out how that affordable housing contribution should be provided.
- 6.5.4 Policy H16 of the Draft London Plan (showing minor suggested changes) states that large-scale purpose-built shared living must deliver a cash in lieu contribution towards conventional C3 affordable housing. Boroughs should use this contribution for the provision of new C3 off-site affordable housing as either;
- a) Upfront cash in lieu payment to the local authority; or
 - b) In perpetuity annual payment to the local authority

In both cases, developments are expected to provide a contribution that is equivalent to 35 percent of the units, to be provided at a discount of 50 percent of the market rent.

- 6.5.5 The applicant has submitted a Financial Viability Assessment to demonstrate how much affordable housing the scheme can deliver. The submitted information has been independently reviewed by an independent viability consultant and tested to ensure that assumptions with regard to affordable housing is the maximum reasonable affordable housing that can be made as part of the proposed scheme. The independent review of the viability assessment has concluded that the assumptions are broadly accurate and that the scheme cannot support an affordable housing contribution.
- 6.5.6 The applicant has calculated the future value that any late stage review could capture, subject to market performance, based on the GLA's late stage review formula. The methodology for the calculation of this offer was based on the viability inputs of the council's independent viability consultant including their opinion on rental growth over the theoretical development plan applying build cost inflation. Based on this, it was calculated that a late stage review would not be likely to capture any affordable housing contribution and would essentially result in a maximum reasonable offer of zero, as per the GLA methodology. Notwithstanding this, the applicant has offered a sum of £393,647 on the basis that the late stage review clauses are not applied in order to reduce uncertainty and to assist the applicant in obtaining project financing. This has been calculated as per the approach adopted by previous granted co-living schemes – The Collective on Palmerston Road (P/2555/18) and more recently development at Lowlands Road (P/1656/20) where Committee Members resolved to grant the scheme, subject to legal agreement.

- 6.5.7 The Planning Policy Team was consulted on this, and considers that as the same approach was utilised for the determination of previous co-living schemes and taking into account that the applicant has proposed an offer which could possibly exceed any affordable housing contribution likely to be captured through a late stage review of a housing typology which is within an emerging market, it is considered to be acceptable. Furthermore, it is considered that the applicant's financial contribution of £393,647 would assist the Council in delivering affordable housing within the borough and would assist in the delivery of mixed and balanced communities within the borough.
- 6.5.8 The Planning Obligations SPD (2013) seeks that for schemes that are below a policy compliant Affordable Housing offer, then a review mechanism ought to be applied. Whilst it is acknowledged that the proposed development is likely to be a single phased development, given the lifespan of the permission (3 years), it is plausible that there could be a change in market circumstances prior to units being rented. Officers therefore consider that an early stage review mechanism should be employed to ensure that in the event that the commencement of the development is delayed, a viability reappraisal can be undertaken to capture any improvement in market conditions that will allow provision for affordable housing contributions to be sought as part of the development. This approach is in line with Policy 3.12 of the London Plan.
- 6.5.9 Notwithstanding the above, given that the upfront contribution put forward by the applicant is considered to exceed what is likely to be captured under a late stage review (based on the GLA methodology), the offer made by the applicant is considered to exceed the maximum reasonable level of affordable housing contribution. Officers are therefore satisfied that this would appropriately offset the requirement for a late stage review mechanism to be applied. The upfront contribution in lieu of the late stage review clause of £393,647 is considered to comply with the policy requirements of policy H16 and will be secured through a planning obligation in the S106 agreement.
- 6.5.10 Subject to the above legal agreement, the proposed development complies with the National Planning Policy Framework (2019), policies 3.12 and 3.13 of The London Plan (2016), policy H16 of The Draft London Plan (2019), policy CS1 of the Core Strategy (2012), policies DM1 and DM50 of the Harrow Development Management Policies Local Plan (2013).

6.6 Traffic, Safety and Parking

- 6.6.1 The relevant policies are:
- The London Plan (2016): 6.3, 6.9, 6.13
 - The Draft London Plan (2019): T4, T5, T6;
 - Harrow Core Strategy (2012): CS1;
 - Harrow Development Management Policies Local Plan (2013): DM1, DM42, DM44
- 6.6.2 This proposal is located within a PTAL 6a area, as such access to public transport is considered to be excellent. Buses, rail and underground services and the main shopping area of Harrow Town Centre are all available within a short walk from the site; the nearest bus stops and shops are 3 minutes away. The site is within Controlled Parking Zone D and there is a public car park opposite.

Car & Cycle Parking

- 6.6.3 Policy T6.1 E of The Draft London Plan (2019) states - Large-scale purpose-built shared living, student accommodation and other sui generis residential uses should be car-free. In accordance with policy T6.1 E the proposed development does not include any standard parking spaces and it would be car-free, to be secured through a S106 Agreement. The proposed level of disabled parking (3 spaces) is in line with the minimum 3% parking required at this location. These are located to the front of the site and accessed via Greenhill Way to ensure safe and easy access to the main entrance of the site.
- 6.6.4 The London Plan (2016) requires 1 space for studios and 1 bedroom units and 2 spaces for all other units. As such, the development requires 90 long stay spaces to comply with London Plan (2016) (4 of which would be adaptable) located to the rear of the site. In addition to the long stay secure cycle parking, The London Plan (2016) also requires that 1 space per 40 units is provided for short stay cycle parking. It is proposed to provide 11 spaces, which exceeds the policy requirements. These spaces are located at the front of the development outside the main entrance to the building. Both locations for short and long stay cycle parking are considered to be appropriate. However, it is considered that details of the proposed secure cycle storage in the rear amenity space as well as the short stay stands are secured by way of condition prior to occupation.

Access & Servicing

- 6.6.5 The site is currently accessible from both Greenhill Way and Greenhill Road, these are primarily tailored for vehicle entry/exit. There is however no through-access between Greenhill Road and Greenhill Way for vehicles. It is proposed to relocate and rationalise the main entrances to the site by creating a single pedestrian entrance off Greenhill Way to the west of the site so that the main vehicle access points are confined to the east. The three proposed disabled spaces would be accessed off Greenhill Way via the existing access point whereas the service and delivery vehicles would use Greenhill Road to avoid obstruction to traffic flow along Greenhill Way when in use. The Highways Officer raises no objection to this arrangement provided that the loading area is not marked as a bay so that it does not encroach on the highway.
- 6.6.6 To facilitate the above, some changes to the existing highway layout would be required. It is proposed to remove the existing footway / kerb outside the Greenhill Road access and resurface this area as a carriageway. On Greenhill Way, the construction of a dropped kerb is proposed to ensure level access into the site. This would also include moving the existing pedestrian guard railings to reduce clutter and improve the pedestrian environment at this location. Materials associated with the changes are proposed to match the existing to achieve seamless transition and a contribution towards additional planting required to replace the removal of the secondary railing.
- 6.6.7 The Highways Officer has acknowledged that there are currently no pedestrian crossing facilities in the immediate vicinity of the site; the nearest is located to the east, approximately 100 metres away and to the west approximately 200 metres away. The accident data has been analysed in the report but not shared however it would seem that a serious incident took place outside the proposal site. Whilst

the assessment does not deem this an indication of any highway layout issues, it is important to note that this proposal is expected to introduce a significant number of people to the location who are expected to walk as their primary mode of travel meaning that it is essential to ensure that sufficient facilities are in place to make this a safe and realistic option. Further clarification was provided as part of an Active Travel Zone (ATZ) assessment which has been deemed acceptable by the Council's Highways Officer. On the basis of the highway works proposed to facilitate the development, the Highways Officer has confirmed that this could be dealt with via Section 278 legal agreement.

6.6.8 The applicant submitted an amended Transport Assessment to provide further details in relation to the swept path analysis and vehicle tracking as well as an amendment to include a third disabled parking bay. The Highways Officer reviewed the amended Transport Assessment and confirmed that it is acceptable and no objections were raised, subject to conditions requiring the submission of a Parking Design and Management Plan as well as a detailed Construction Logistics Plan in accordance with Transport for London Guidance. It is therefore considered that the proposed development would be acceptable in Highway terms and would accord with policies 6.3, 6.9B and 6.13 of The London Plan (2016), policies T5, T6, T6.1 of The Draft London Plan (2019) and policies DM1 and DM42 of the Harrow Development Management Policies Local Plan (2013).

6.7 Development and Flood Risk

6.7.1 The relevant policies are:

- The London Plan (2016): 5.12, 5.13
- The Draft London Plan (2019): SI12, SI13
- Harrow Core Strategy (2012): CS1
- Harrow Development Management Policies Local Plan (2013): DM1, DM9, DM10

6.7.2 The site is not located within a flood zone. However, is located within a Critical Drainage Area and given the potential for the site to result in higher levels of water discharge into the surrounding drains, could have an impact on the capacity of the surrounding water network to cope with higher than normal levels of rainfall.

6.7.3 A Flood Risk Assessment and Drainage Strategy was submitted in support of the application. This was reviewed by the Council's Drainage Engineer and deemed to be satisfactory. However, the proposed development would be required to submit further information in relation to connections from the site to Thames Water Connections within the public realm. Furthermore, details in relation to Floodwater Disposal, Surface Water Disposal and Storm Water Attenuation would need to be provided. However, the Drainage Authority consider that subject to safeguarding conditions requiring this information prior to commencement, the scheme would be broadly acceptable and would not result in flood risk within the area. On this basis, appropriate conditions have been recommended.

6.7.4 Subject to the above, the proposal therefore complies with the National Planning Policy Framework (2019), policies 5.12 and 5.13 of The London Plan (2016), policy SI12 and SI13 of The Draft London Plan (2019), policy CS1 of the Core Strategy (2012) and policies DM1, DM9 and DM10 of the Harrow Development

6.8 Ecology and Biodiversity

6.8.1 The relevant policies are:

- NPPF: Chapter 15
- London Plan: 7.19
- Harrow Development Management Local Policies: DM20 and DM21
- Draft London Plan: G6

6.8.2 The applicant has submitted a Preliminary Ecological Appraisal (PEA) in support of this application. The site is more than 700 metres distance from the closest Site of Importance for Nature Conservation (SINC)-designated areas and it is not anticipated that the scheme would have any direct impact on these. The development is in an area where there is an identified need to improve green infrastructure and respond to the deficit in access to nature for local residents, particularly given the number of additional residents it is proposed to house in this location. The application site is located within a predominantly urbanised area and predominantly hardstanding, with no recognised biodiversity or ecological value, however the PEA recommends a contribution towards biodiversity. On the basis of the existing site circumstances which primarily comprise hard standing, the Council's Biodiversity Officer has recommended that conditions requiring the submission of a detailed plan of enhancement be included.

6.8.3 Subject to the conditions recommended above, it is considered that the proposal development would meet the aspirations of the policies listed under paragraph 6.8.1 above.

6.9 Energy and Sustainability

6.9.1 The relevant policies are:

- London Plan: 5.1, 5.2, 5.15,
- Draft London Plan: SI 1, SI 5
- Harrow Core Strategy: CS1 T
- Harrow Development Management Local Policies: DM1, DM12 and DM14

6.9.2 In broad terms, under Policy 5.2 of the current London Plan, all development is expected to achieve at least a 35% reduction in carbon emission through on-site measures. Any remaining residential carbon emissions are to be offset by way of monetary contribution to ensure that the residential element of the development is zero carbon.

6.9.3 The applicant has submitted an Energy & Sustainability Statement (Integration, May 2020). On review, the content has been found to generally accord with the requirements of the current and 'Intend to Publish' London Plan in that a minimum 35% carbon reduction is proposed on site (35.9%) with the balance carbon emissions (residential element at 36.48 tonnes) to be offset through monetary contribution (£65,672) to achieve the required zero carbon residential development.

- 6.9.4 Within the 35.9% reduction on-site, 9.7% will be through energy efficiency measures (residential element), broadly meeting the proposed 10% in the 'Intend to Publish' London Plan. The energy efficiency savings from the non-residential element is 6.0%, which is below the 15% requirement of the 'Intend to Publish' London Plan and is not ideal, however this only forms a relatively small part of the scheme. The statement identifies that there are no district heat networks capable of serving the site, but the provision of an on-site communal heating network will enable future connection should a district network become available; this is acceptable. Furthermore, a 26.2% reduction is achieved through solar PV, which is reasonable given constraints with respect to roof space.
- 6.9.5 It is recommended that planning obligations are secured via Section 106 agreement which safeguard a route for future connection to a district heat network; carbon offset contribution (£65,672) prior to commencement of development and a post-construction assessment to determine if the proposed carbon reductions have been achieved and whether an additional offset contribution (residential element) is required to offset any shortfall, calculated at £1,800 per tonne of carbon. In addition, a condition requiring that the development is constructed and operated generally in accordance with the energy strategy submitted with the application has been added.
- 6.9.6 Subject to the above obligation and condition, the proposed development complies with the National Planning Policy Framework (2019), policies 5.2, 5.13 and 5.15 of The London Plan (2016), policy SI1 and SI5 of The Draft London Plan (2019), policy CS1 of the Core Strategy (2012), policies DM1, DM12 and DM14 of the Harrow Development Management Policies Local Plan (2013).

6.10 Accessibility

- 6.10.1 The relevant policies are:
- London Plan: 3.5, 7.2
 - The Draft London Plan (2019): D5
 - Harrow Core Strategy: CS1
 - Harrow Development Management Local Policies: DM1, DM2
- 6.10.2 The proposed scheme would provide 89 co-living units out of which 9 units would be designed wheelchair accessible. These are spread across the development; 2 x ground floor; 3 x first floor and 2 x third floor. This would equate to approximately 10% which would accord with the London Plan policy. There would be inclusive levelled access across the site and three disabled parking spaces would be provided.
- 6.10.3 Officers are satisfied that the proposals meet the development plan objectives in terms of accessibility and inclusive design and would contribute to the creation of a 'lifetime neighbourhood'. On this basis, it is considered that the proposal complies with the high quality design aspirations of the National Planning Policy Framework (2019), policies 3.5 and 7.2 of The London Plan (2016), policy D5 of The Draft London Plan (2019), policy CS1 of the Core Strategy (2012), policies DM1 and DM2 of the Harrow Development Management Policies Local Plan (2013) and the Supplementary Planning Document: Residential Design Guide (2010).

7.0 CONCLUSION AND REASONS FOR APPROVAL

- 7.1 The proposed development would deliver additional housing of a satisfactory layout and design to ensure that the future occupiers would benefit from a high standard of living accommodation. The redevelopment of the site would enhance the urban environment in terms of material presence, attractive streetscape, and accessible routes in and out of the site and would make a positive contribution to the local area, in terms of quality and character. The massing and scale proposed would appropriately relate to the wider area and would permit full optimisation of this previously developed land to bring forward much needed housing which would positively add to the Council's housing delivery targets. Accordingly, the development would accord with development plan policies and is recommended for approval.

APPENDIX 1: Conditions and Informatives

Conditions

1. Full Time

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON: To comply with the provisions of Section 91 of the Town and Country Planning Act 1990.

2. Approved Plans:

L(02) 620 Rev P1, L(02) 600 Rev P1, L(02) 601 P1, L(01) 100 P05, L(01) 101 P2, L(01) 102 Rev P2, L(01) 103 Rev P3, L(01) 104 Rev P2, L(01) 106 Rev P2, L(01) 107 Rev P05, L(01) 108 Rev P03, A1 19E059 Rev 001, L (01) 001 Rev P1, 4614-DR01 Rev C, L (01) 05 Rev P1, L (03) 100 Rev P3 L (03) 101 Rev P3, L (03) 102 Rev P2, L (02) 101 Rev P6, L (02) 102 Rev P6, L (02) 103 Rev P6, L (02) 104 Rev P3, L (02) 106 Rev P4, L (02) 107 Rev P06, L (02) 108 Rev P04, L (02) 109 Rev P3, L (90) 100 Rev P2, L (90) 101 Rev P2, L (01) 109 Rev P1, L (01) 110 Rev P01, Private Suite Configuration, Accessible Suite Information, Core Features, L(02) 115, L(02)116, L(02) 117, Waste Management Strategy Note Vehicle Access and Waste Management Strategy Note, Schedule of Areas, Response to COVID 19, Proposed scheme vs. Approved Residential Scheme, Dandi Profile, Active Travel Zone Assessment Management and Operation Plan, Planning Statement (May 2020); Daylight & Sunlight Report (18th March 2020), Design and Access Statement, Energy and Sustainability Statement (May 2020); Surface water drainage strategy and FRA, Transport Assessment (May 2020), Travel Plan (January 2020), Waste Management Strategy (March 2020), Noise Impact Assessment, Document Verification (Ground and Project Consultants), Preliminary Ecological Appraisal , Harrow HMO Demand Document, Community Benefits Document, Greenhill Way - Fire Strategy, Co-living prepared by Dandi document

3. Construction Logistics Plan

No development shall take place, including any works of demolition, until a Detailed Construction Logistics Plan has been submitted to, and approved in writing by, the local planning authority in accordance with the format and guidance provided by the Transport for London – www.constructionlogistics.org. The Detailed Construction Logistics Plan shall provide for:

- a) Parking of vehicles of site operatives/visitors;
- b) HGV access to site – loading and unloading of plant and materials;
- c) Number of HGV's anticipated;
- d) Storage of plant and materials used in constructing the development;
- e) Programme of work and phasing (in line with the Phasing Strategy to be submitted pursuant to condition 3 attached to this permission);
- f) Site layout plan;
- g) Highway condition (before, during, after);
- h) Measures to control dust and dirt during construction;
- i) A scheme for recycling/disposing of waste resulting from demolition and construction works;
- j) details showing the frontage/ the boundary of the site enclosed by site hording to a minimum height of 2 metres; and
- k) Details of cranes and other tall construction equipment (including obstacle lighting).

The development shall be carried out in accordance with the approved Detailed Construction Logistics Plan, or any amendment or variation to it as may be agreed in writing by the local planning authority.

REASON: To minimise the impacts of construction upon the amenities of neighbouring occupiers and to ensure that development does not adversely affect safety on the transport network. Details are required PRIOR TO COMMENCEMENT OF DEVELOPMENT to ensure a satisfactory method of construction is agreed prior to any works on site commencing.

4. Sewage Disposal

The development hereby permitted shall not be commenced until works for the disposal of sewage have been provided on site in accordance with details to be submitted to, and approved in writing by, the local planning authority. The development shall be carried out in accordance with the details as agreed and shall thereafter be retained.

REASON: To ensure that adequate drainage facilities are provided in accordance with Sewers for Adoption. This is a PRE-COMMENCEMENT condition.

5. Surface Water Disposal

The development of any buildings hereby permitted shall not be commenced until works for the disposal of surface water, surface water attenuation and storage works have been submitted to, and approved in writing by, the local planning authority. Details shall include full specification of the permeable paving and details to the long-term maintenance and management of the on-site drainage. The development shall be carried out in accordance with the details as agreed and shall thereafter be retained.

REASON: To ensure that adequate drainage facilities are provided, reduce and mitigate the effects of flood risk following guidance in the National Planning Policy Framework. This is a PRE-COMMENCEMENT condition.

6. Site Levels

No site works or development shall commence until details of the levels of the building(s), road(s) and footpath(s) in relation to the adjoining land and highway(s), and any other changes proposed in the level of the site, have been submitted to, and agreed in writing by the local planning authority. The development shall be carried out in accordance with the details so agreed.

REASON: To ensure that the works are carried out at suitable levels in relation to the highway and adjoining properties in the interests of the amenity of neighbouring residents, the appearance of the development, drainage, gradient of access and future highway improvement. This is a PRE-COMMENCEMENT condition.

7. Contamination

No demolition shall take place until a scheme for identifying, managing and disposing of any potential contamination hazards found during demolition of the existing buildings and structures on the site has first been submitted to, and agreed in writing by, the local planning authority. No development other than demolition shall take place until a scheme ('the second scheme') for the management of contamination risk at the site has first been submitted to, and agreed in writing by, the local planning authority. The second scheme shall include the following:

- (a) details of a site investigation to provide information for a detailed assessment of the risks to all receptors that may be affected, including those off site;
- (b) the results of the site investigation and an options appraisal and remediation strategy giving full details of remediation measures and how they are to be undertaken; and
- (c) a verification plan providing details of the data that will be collected in order to demonstrate that the works set out in the remediation strategy are complete and identifying any requirements for longer-term monitoring of pollutant leakages, maintenance and arrangements for contingency action.

The demolition shall be carried out in accordance with the first scheme so agreed. The development other than demolition shall be carried out in accordance with the second scheme so agreed.

REASON: To ensure that the development does not activate or spread potential contamination at the site and that the land is appropriately remediated for the approved uses.

8. Materials

Notwithstanding the details shown on the approved drawings, the development hereby approved shall not progress beyond damp proof course level until samples of the materials (or appropriate specification) to be used in the construction of the external surfaces noted below have been submitted to, and agreed in writing by, the local planning authority:

- a) facing materials for the building, including brickwork bond details;
- b) windows/ doors, including those to all servicing areas and reveals;
- c) balcony screens including balustrade detail, privacy screens and soffits;
- d) boundary treatment including all vehicle and pedestrian/ access gates;
- e) ground surfacing; and
- f) raised planters;
- g) Rainwater disposal systems (including downpipes) and soil stacks;

The development shall be carried out in accordance with the details so agreed and shall be retained as such thereafter.

REASON: To safeguard the appearance of the locality and to ensure a satisfactory form of development.

9. Detailed Ecological Plan

The development hereby approved shall not progress beyond damp proof course level until the following details has been submitted to, and agreed in writing by the local planning authority:

- a) a fully detailed plan, complete with any necessary illustrations, elevations, plans and specifications, for the provision of biodiversity gain within the development, to encompass built structures and soft landscaping elements, including but not limited to the suggestions within the Ambient PEA.

This document, which should be prepared by a suitable qualified and experienced ecologist, shall take account of the context of the local area, the Harrow biodiversity action plan, priority species found within the borough. It shall include shelter provision for bats, swifts, house sparrows and other urban/suburban bird species, as well as for invertebrates; the provision of green/blue roofs, sections of green wall and other 3d structures within the gardens, terraces and elsewhere, ensuring that landscaping elements maximise

the benefits they provide for pollinators and other wildlife in or likely to be attracted to the area as a result of improvements to green infrastructure. It will include a time plan in order to ensure the effective incorporation of the identified measures within the overall delivery of the proposed development.

- b) the applicant or their consultants will provide to Greenspace Information for Greater London for their future use, in a format to be agreed with Harrow Council, all species records (including recorder(s) names, scientific species name, abundance information, date, and full resolution location data etc.) and Phase 1 mapping information collected in support of this application.
- c) Notwithstanding any grant of planning permission, the applicant will ensure the delivery of the measures approved under (a) during the construction phase is in accordance with the agreed time plan this provides.

REASON: For the purposes of proper planning, to provide certainty and to ensure sustainable development in accordance with the requirements of national, regional and local plan policies relating to the protection and enhancement of biodiversity.

10. Biodiversity Management Plan

The development shall not progress beyond damp proof course level until detailed proposals for ongoing management, monitoring and reporting of onsite biodiversity features, including details of how this will be funded in perpetuity have been submitted to and approved in writing by the Local Planning Authority. The Biodiversity Management Plan shall include:

- (a) how these features will be managed and monitored to maintain and enhance their value, including any replacements during the establishment phase (to run for three years from the commencement of construction);
- (b) year by year schedule of the timing of operations and who has responsibility for these and for ensuring they are undertaken;
- (c) how the plan will be updated at 5 year intervals; and
- (d) how these operations will be funded over the lifetime of the new development, during and following construction.

The development shall be carried out in accordance with the proposals so agreed and shall be retained as such thereafter.

REASON: To ensure that the development makes appropriate provision for the protection, enhancement, creation and management of biodiversity within the site and surrounding area.

11. Construction Management Strategy

No development shall take place, including any works of demolition, until a demolition and construction logistics plan has first been submitted to and agreed in writing by the Local Planning Authority. The plan shall detail the arrangements for:

- a) the parking of vehicles of site operatives and visitors;
- b) loading and unloading of plant and materials;
- c) storage of plant and materials used in construction of the development;
- d) the erection and maintenance of security hoardings;
- e) measures for the control and reduction of dust, noise and vibration

The demolition and construction of the development shall be carried out in accordance with the plan so agreed.

Reason: To ensure that measures are put in place to ensure the transport network impact of demolition and construction work associated with the development is managed and reduce noise and vibration impacts during demolition and construction and to safeguard the amenity of neighbouring occupiers. Details are required prior to commencement of development to ensure a satisfactory form of development.

12. Mechanical Ventilation

The use hereby approved shall not commence until detailed ventilation and extraction system(s) have been installed in accordance with details that shall have first been submitted to, and agreed in writing by, the local planning authority. The details shall include the discharge output of the system(s), the methods for attenuating noise and vibration from the system(s), and the siting and external appearance of the system(s). The systems so agreed shall thereafter be retained in connection with the use hereby approved.

No soil stacks, soil vent pipes, flues, ductwork or any other pipework shall be fixed to the elevations of the buildings hereby approved, unless otherwise approved by the Local Planning Authority.

REASON: To ensure that any noise and vibration impacts of the use are appropriately mitigated, and to ensure that the external siting and external appearance of the ventilation and extraction system(s) achieve a high standard of design and amenity.

13. External Lighting

Notwithstanding the approved details, the development hereby permitted shall not progress beyond damp proof course level, until details of the lighting of all external communal areas and other external areas (including buildings) within the site has been submitted to the Local Planning Authority. This includes all details of the lighting including sourcing/ manufacturer details, location, height type, direction of light sources, specification, elevations, light spillage and lighting levels The

development shall be implemented in accordance with the approved details and shall be retained as such thereafter.

REASON: To ensure that the development incorporates lighting that contributes to Secured by Design principles, achieves a high standard of quality.

14. Hard and Soft Landscaping

Notwithstanding the approved details, the development hereby permitted shall not progress beyond damp proof course level, until details of soft and hard landscaping for the site has been submitted to, and approved in writing by the Local Planning Authority which shall include:

a) A scheme for detailed hard and soft landscaping of the development, to include details of the planting, hard surfacing materials, raised planters and external seating and so on. Soft landscaping works shall include: planting plans (at a scale not less than 1:100), written specification of planting and cultivation works to be undertaken and schedules of plants, noting species, plant sizes, plant container sizes (all at time of planting) and proposed numbers / densities and an implementation programme. Planting which may provide benefits in terms of improving air quality are encouraged. The hard-surfacing details shall include samples to show the texture and colour of the materials to be used and information about their sourcing/manufacturer. The hard and soft landscaping details shall demonstrate how they would contribute to privacy between the communal garden/open space areas and the adjacent co-living units;

b) Green roofs, hard and soft landscape details and planting plans, the roofs at first, third and fourth floor and the roof area with PV panels to be incorporated into the green roof areas where feasible, including written specification of the planting and the biodiverse roof details, planting plans, and associated features (e.g. invertebrate shelters), details of the proposed irrigation or any watering system to be installed for regular watering in dry / drought conditions for the landscaped areas maintenance and proposed ongoing plant replacement, for any plant failures, during the lifetime of the built development;

c) Full scale metric cross sections and elevations for all communal open amenity spaces (at a scale of not less than 1:100) including the proposed details for level changes; and

d) Details of all furniture, boundary treatment, specification for the proposed supports and fixings for plants, landscape structures and any pergolas and climbing plant frames, including proposed material and source / manufacturer, irrigation for planting and detailed drawings of such; for all communal areas, raised beds and bespoke furniture.

The development shall be implemented in accordance with the scheme so agreed and shall be retained as such thereafter.

REASON: To ensure that the development makes provision for hard and soft landscaping which contributes to the creation of a high quality, accessible, safe and attractive public realm and to ensure a high standard of design, layout and amenity.

15. Landscape Management Plan

Notwithstanding the approved details, the development hereby permitted shall not progress beyond damp proof course level, until a Landscape Management Plan and Landscape Maintenance Plan for the site has been submitted to, and approved in writing by the Local Planning Authority to ensure the future success of the development, which shall include long term design objectives, management responsibilities and maintenance schedules, including the lower ground level hard and soft landscape, outdoor terraces, green roofs and any biodiverse roofs and landscape structures for plant growth and a programme of maintenance including a calendar of routine physical tasks for all landscape areas / plant replacement for the life time of the development for landscape structures. The Landscape Maintenance Plan shall cover a 5 year period for the whole of the proposed development to ensure the future success of the development including all the hard and soft landscape.

The development shall be implemented in accordance with the scheme so agreed and shall be retained as such thereafter.

REASON: To ensure that the development makes provision for hard and soft landscaping which contributes to the creation of a high quality, accessible, safe and attractive public realm and to ensure a high standard of design, layout and amenity.

16. Access Strategy

The development hereby approved shall not progress beyond damp proof course level until an inclusive access strategy for the site has been submitted to, and agreed in writing by, the Local Planning Authority. The strategy shall:

- (a) demonstrate inclusive access within the proposed development including all communal areas;
- (b) detail the arrangements for disabled residents' access to the disabled parking spaces and the access, and use of, waste and recycling facilities within the development.

The development shall be carried out in accordance with the approved details prior to the first occupation of the site and shall be retained as such thereafter.

REASON: To ensure that the development contributes to the achievement of a lifetime neighbourhood.

17. Communal Aerials

Prior to the first occupation of the residential units hereby permitted, details of a strategy for the provision of communal facilities for television reception (eg. aerials, dishes and other such equipment) shall be submitted to and approved in writing by

the local planning authority. Such details shall include the specific size and location of all equipment. The approved details shall be implemented prior to the first occupation of the residential units and shall be retained thereafter. No other television reception equipment shall be installed on the building without the prior written approval of the Local Planning Authority.

REASON: To ensure that any telecommunications apparatus and other plant or equipment that is required on the exterior of the buildings preserves the high quality design of the buildings and spaces.

18. Secure By Design

Prior to the first occupation of the development, evidence of Secured by Design Certification shall be submitted to the Local Planning Authority in writing to be agreed, or justification shall be submitted where the accreditation requirements cannot be met. Secure by design measures shall be implemented and the development shall be retained in accordance with the approved details.

REASON: In the interests of creating safer and more sustainable communities and to safeguard amenity by reducing the risk of crime and the fear of crime.

19. Car Parking

The residential units hereby permitted shall not be occupied until three accessible parking spaces of a adequate size have been clearly marked out on site, in accordance with the approved ground floor plan. Such spaces shall not be used for any purposes other than for the parking of motor vehicles used by residents of the development for blue badge holders/disabled persons only, and for no other purpose, unless otherwise agreed in writing by the Local Planning Authority.

REASON: To ensure car parking provision is available for use by the occupants of the site.

20. Cycle Parking

The residential units hereby permitted shall not be occupied until details for secured, sheltered and accessible storage for the provision of cycle spaces have been submitted and approved in writing by the local planning authority. Such cycle storage should be designed in accordance with the requirements of the London Cycle Design Standards. The development shall be completed in accordance with the approved details prior to the occupation of the residential units, and shall thereafter be retained.

REASON: To ensure that cycle storage is available for use by the occupants of the site.

21. Audio Visual Control

The development hereby approved shall not be occupied until: (i) an audio-visual access control system has been installed; or (ii) such alternative security measures have been installed that shall first have been submitted to, and agreed in writing by, the local planning authority. The development shall be completed in accordance with the approved details prior to the first occupation of the development and shall thereafter be retained in that form.

REASON: To ensure that the development achieves a high standard of residential quality for future occupiers of the development.

22. Refuse Stores

The refuse bins shall be stored at all times, other than on collection days, in the designated refuse collection area, as shown on the approved drawing plans.

REASON: To safeguard the appearance and character of the surrounding area, in accordance with policy 7.4.B of The London Plan (2016) and ensure a high standard of residential quality.

23. Permitted Development Restrictions – Communications

Notwithstanding the provisions of Part 16 (Communications) to Schedule 2 of the Town and Country Planning (General Permitted Development) Order 2015, or any order revoking and replacing that Order with or without modification, no development that would otherwise be permitted by that part of the Order (or the equivalent provisions of any replacement Order) shall be carried out without planning permission having first been obtained by the Local Planning Authority.

REASON: To ensure that the architectural integrity of the building is not compromised.

24. Noise and Vibration

The noise and vibration mitigation measures shall be carried out in accordance with the Noise Impact Assessment hereby approved and shall be retained in that form thereafter.

REASON: In order to safeguard the amenities of neighbouring residents and future occupiers.

25. Landscape Implementation

All planting, seeding or turfing comprised in the approved details of landscaping shall be carried out in the first planting and seeding seasons following the occupation of the building(s), or the completion of the development, whichever is the sooner. Any existing or new trees or shrubs which, within a period of 5 years

from the completion of the development, die, are removed, or become seriously damaged or diseased, shall be replaced in the next planting season, with others of a similar size and species, unless the Local Planning Authority agrees any variation in writing.

REASON: To safeguard the appearance and character of the area, and to enhance the appearance of the development.

26. Mailboxes

The mail boxes as shown on the approved plans shall be installed to ensure that they secure and meet all safety standards and shall be made available prior to the first occupation of each relevant building.

REASON: To ensure that the development contributes to the achievement of a lifetime neighbourhood and a high standard of design and layout.

27. Amplification of sound

No music or any other amplified sound caused as a result of this permission shall be audible at the boundary of any residential premises from any of the external terraces above ground floor level, either attached to, or in the vicinity of, the premises to which this permission refers.

REASON: To ensure that the proposed development does not give rise to noise nuisance to neighbouring residents.

28. Fire Strategy

The development hereby approved shall be carried out in accordance with the measures outlined within Greenhill Way - Fire Strategy. The development shall be carried out in accordance with the strategy so agreed and shall be retained as such thereafter unless otherwise agreed by the local planning authority or Fire Authority.

REASON: To ensure that the development proposals achieve the highest standard of fire safety.

Informatives

1. The following policies are relevant to this decision:

National Planning Policy Framework (2019)

The London Plan (2016):

2.13, 2.15, 3.3, 3.4, 3.5C, 3.8, 3.9, 3.11, 3.12, 3.13, 4.12, 5.2, 5.3, 5.6, 5.7, 5.9, 5.10, 5.12, 5.13, 5.17, 5.21, 6.3, 6.9, 6.13, 7.1, 7.2, 7.3B, 7.4B, 7.5, 7.6B, 7.7, 7.8, 7.13, 7.14, 7.15, 7.19, 7.21

The Draft London Plan (2019):

D1, D2, D3, D4, D5, D6, D7, D8, D9, D11, D12, D13, H1, H2, H5, H8, H12, H16, HC1, G5, G6, G7, SI1, SI2, SI3, SI5, SI8, SI13, T2, T3, T4, T5, T6, SD6, SD7

Harrow Core Strategy (2012):

CS1

Harrow and Wealdstone Area Action Plan (2013)

AAP1, AAP4, AAP5, AAP6, AAP8, AAP13, AAP19

Harrow Development Management Policies Local Plan (2013):

DM1, DM2, DM9, DM10, DM12, DM13, DM14, DM15, DM20, DM21, DM22, DM23, DM27, DM42, DM43, DM44, DM45, DM50

Supplementary Planning Documents:

Supplementary Planning Document: Residential Design Guide (2010)

Supplementary Planning Document: Planning Obligations and Affordable Housing (2013)

Technical Housing Standard (2015)

Code of Practice for Storage and Collection of Refuse and Materials for Recycling in Domestic Properties (2008).

Mayors Housing Supplementary Planning Guidance (2016)

2. Grant without Pre-application

Statement under Article 35(2) of The Town and Country Planning (Development Management Procedures) (England) Order 2015

This decision has been taken in accordance with paragraphs 39-42 of The National Planning Policy Framework. Harrow Council has a pre-application advice service and actively encourages applicants to use this service. Please note this for future reference prior to submitting any future planning applications.

3. Considerate Contractor Code of Practice

The applicant's attention is drawn to the requirements in the Considerate Contractor Code of Practice. In the interests of minimising any adverse effects arising from building operations, the limitations on hours of working are as follows:

0800-1800 hours Monday - Friday (not including Bank Holidays)

0800-1300 hours Saturday

4. Party Wall Act

The Party Wall etc. Act 1996 requires a building owner to notify and obtain formal agreement from adjoining owner(s) where the building owner intends to carry out building work which involves:

1. work on an existing wall shared with another property;
 2. building on the boundary with a neighbouring property;
 3. excavating near a neighbouring building,
- and that work falls within the scope of the Act.

Procedures under this Act are quite separate from the need for planning permission or building regulations approval.

"The Party Wall etc. Act 1996: explanatory booklet" is available free of charge from: Communities and Local Government Publications, PO Box 236, Wetherby, LS23 7NB Please quote Product code: 02 BR 00862 when ordering.

Also available for download from the Portal website:

<https://www.gov.uk/party-wall-etc-act-1996-guidance>

5. Compliance with planning conditions

IMPORTANT: Compliance With Planning Conditions Requiring Submission and Approval of Details Before Development Commences

- You will be in breach of planning permission if you start development without complying with a condition requiring you to do something before you start. For example, that a scheme or details of the development must first be approved by the Local Planning Authority.
- Carrying out works in breach of such a condition will not satisfy the requirement to commence the development within the time permitted.
- Beginning development in breach of a planning condition will invalidate your planning permission.
- If you require confirmation as to whether the works you have carried out are acceptable, then you should apply to the Local Planning Authority for a certificate of lawfulness.

6. Mayoral CIL Payment

Please be advised that approval of this application (either by Harrow Council, or subsequently by the Planning Inspectorate if allowed on appeal following a refusal by Harrow Council) will attract a Community Infrastructure Levy (CIL) liability, which is payable upon the commencement of development. This charge is levied under s.206 of the Planning Act 2008 Harrow Council, as CIL collecting authority, has responsibility for the collection of the Mayoral CIL The Provisional Mayoral CIL liability for the application, based on the Mayoral CIL levy rate for Harrow of £60/sqm is £188,820.

The floorspace subject to CIL may also change as a result of more detailed measuring and taking into account any in-use floor space and relief grants (i.e. for example, social housing).

You are advised to visit the [planningportal](https://ecab.planningportal.co.uk) website where you can download the appropriate document templates.

Please complete and return the Assumption of Liability Form 1 and CIL Additional Information Form 0 .

https://ecab.planningportal.co.uk/uploads/1app/forms/form_1_assumption_of_liability.pdf

https://ecab.planningportal.co.uk/uploads/1app/forms/cil_questions.pdf

If you have a Commencement Date please also complete CIL Form 6:

https://ecab.planningportal.co.uk/uploads/1app/forms/form_6_commencement_notice.pdf.

The above forms should be emailed to HarrowCIL@Harrow.gov.uk

Please note that the above forms must be completed and provided to the Council prior to the commencement of the development; failure to do this may result in surcharges and penalties

7. Liability for damage to highway

The applicant is advised to ensure that the highway is not interfered with or obstructed at any time during the execution of any works on land adjacent to a highway. The applicant is liable for any damage caused to any footway, footpath, grass verge, vehicle crossing, carriageway or highway asset. Please report any damage to nrswa@harrow.gov.uk or telephone 020 8424 1884 where assistance with the repair of the damage is available, at the applicants expense. Failure to report any damage could result in a charge being levied against the property.

8. Street Numbering

Harrow Council is responsible for the naming and numbering of new or existing streets and buildings within the borough boundaries. The council carries out these functions under the London Government Act 1963 and the London Building Acts (Amendment) Act 1939.

All new developments, sub division of existing properties or changes to street names or numbers will require an application for official Street Naming and Numbering (SNN). If you do not have your development officially named/numbered, then then it will not be officially registered and new owners etc. will have difficulty registering with utility companies etc.

You can apply for SNN by contacting technicalservices@harrow.gov.uk or on the following link.

http://www.harrow.gov.uk/info/100011/transport_and_streets/1579/street_naming_and_numbering

9. Sustainable Drainage Systems

The applicant is advised that surface water run-off should be controlled as near to its source as possible through a sustainable drainage approach to surface water management (SUDS). SUDS are an approach to managing surface water run-off which seeks to mimic natural drainage systems and retain water on or near the site as opposed to traditional drainage approaches which involve piping water off site as quickly as possible.

SUDS involve a range of techniques including soakaways, infiltration trenches, permeable pavements, grassed swales, ponds and wetlands. SUDS offer significant advantages over conventional piped drainage systems in reducing flood risk by attenuating the rate and quantity of surface water run-off from a site, promoting groundwater recharge, and improving water quality and amenity.

Where the intention is to use soak ways they should be shown to work through an appropriate assessment carried out under Building Research Establishment (BRE) Digest 365.

Support for the SUDS approach to managing surface water run-off is set out in the National Planning Policy Framework (NPPF) and its accompanying technical guidance, as well as the London Plan. Specifically, the NPPF (2012) gives priority to the use of sustainable drainage systems in the management of residual flood risk and the technical guidance confirms that the use of such systems is a policy aim in all flood zones. Policy 5.13 of the London Plan (2016) requires development to utilise sustainable drainage systems unless there are practical reasons for not doing so.

Sustainable drainage systems cover the whole range of sustainable approaches to surface drainage management. They are designed to control surface water run-off close

to where it falls and mimic natural drainage as closely as possible. Therefore, almost any development should be able to include a sustainable drainage scheme based on these principles.

The applicant can contact Harrow Drainage Section for further information.

10. Design out crime

The applicant is advised to engage with the Designing Out Crime Officer at an early stage of the detailed design stage in respect of meeting the requirement of Secured by Design condition.

11. Network Management

The developer is urged to make early contact with Network Management in order to agree any temporary traffic management measures required; these should then be included in the detailed CLP for submission.

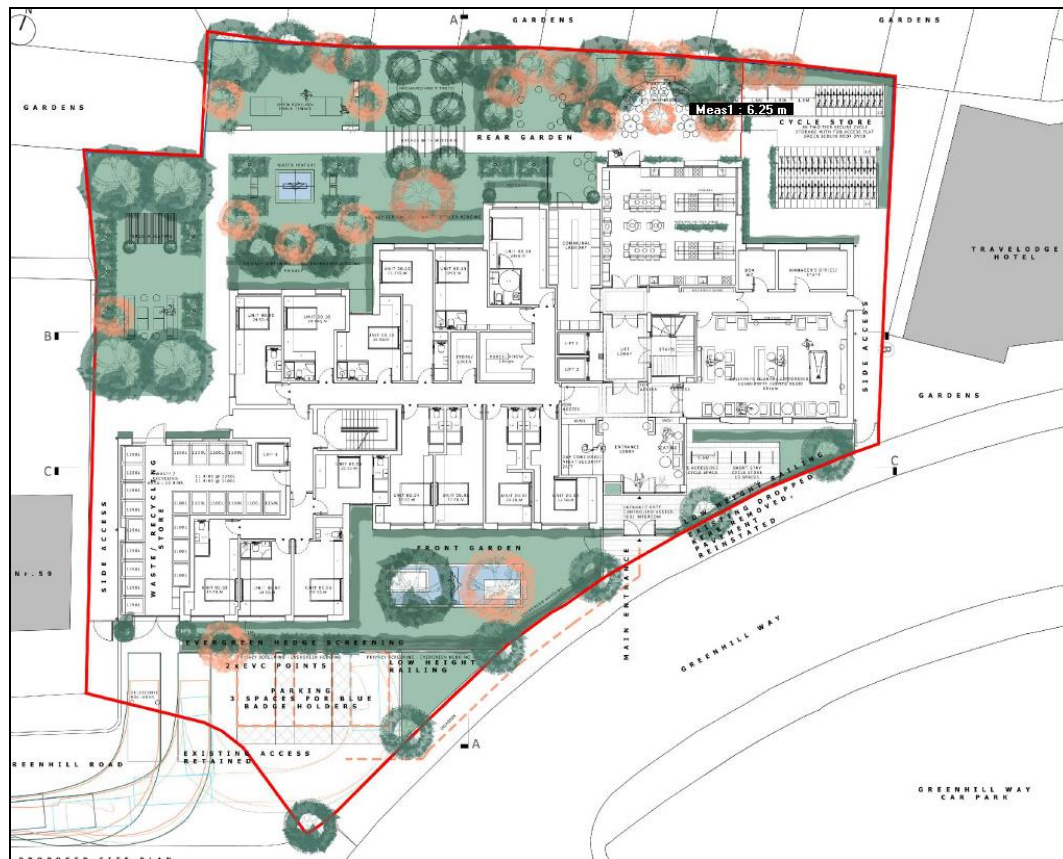
Checked

Interim Chief Planning Officer	Orla Murphy pp Beverley Kuchar
Corporate Director	Paul Walker 1/10/2020

APPENDIX 2: SITE PLAN



Existing Site Plan (above) & Proposed Site Plan (below)



APPENDIX 3: SITE PHOTOS



View towards Debenhams and Harrow Town Centre from Application Site



View towards the rear of properties on Fairholme Road & Travelodge (right)



View towards Travelodge (West of the site)



View of Greenhill Way and Application Site from Greenhill Way Car Park opposite



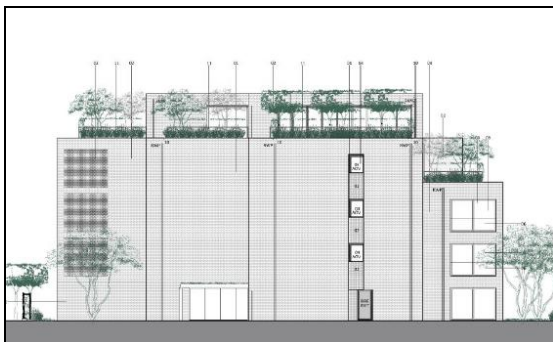
View from Application Site towards No. 59 Greenhill Road and No. 2 Greenhill Way

APPENDIX 4: PLANS & ELEVATIONS

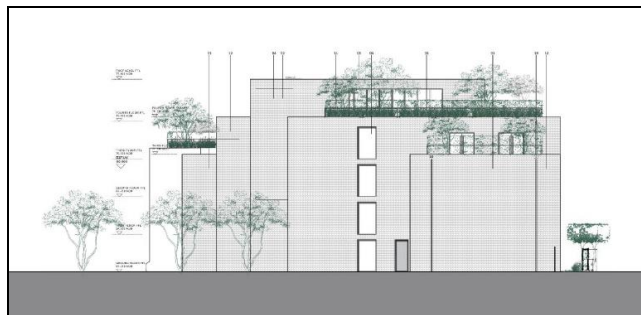
Proposed Front Elevation



Proposed Rear Elevation



Proposed East Elevation



Proposed West Elevation

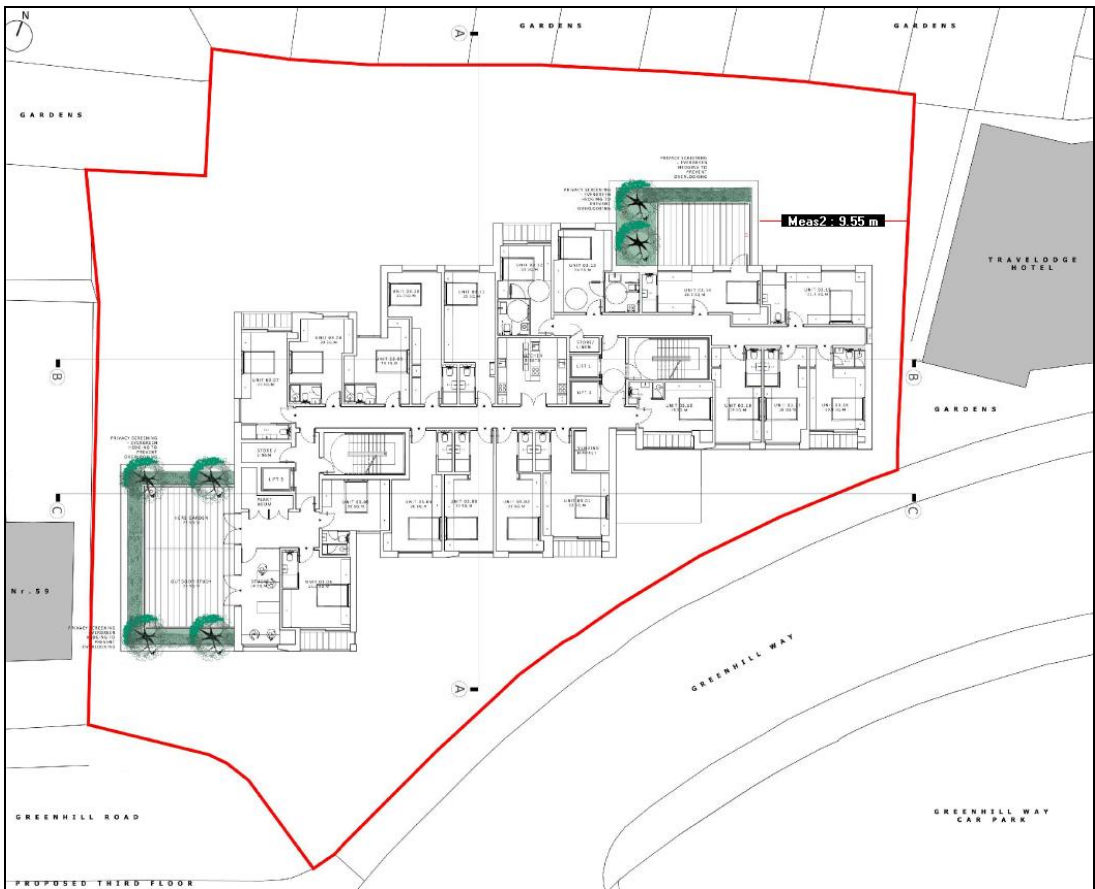


Proposed Ground Floor Plan (Above) & Proposed First Floor Plan (Below)





Proposed Second Floor (above) & Proposed Third Floor (Below)





Proposed Fourth Floor Plan (above) & Proposed Street Scene (Below)

